



CITY OF HOOD RIVER

PLANNING DEPARTMENT

211 2nd Street, Hood River, OR 97031 Phone: 541-387-5210

STAFF REPORT LANDMARKS REVIEW – NEW CONSTRUCTION

July 9, 2020

Application submitted: November 23, 2019
Application complete: May 26, 2020
120-day deadline: September 23, 2020

To: Landmarks Review Board
From: Jennifer Kaden, Associate Planner
RE: File No. 2020-02 – Landmarks Review for a New Food Cart Park & Associated Site Improvements and for an Addition to an Existing Building located on a Site in the Downtown Historic District

I. BACKGROUND INFORMATION:

- A. **REQUEST:** Landmarks Review request to construct a new food cart park with up to 3 food carts, with patio, outdoor seating & fire pit covered by a pergola and sail-shades; construct an addition to an existing building including a restroom and service window; and permit 3 existing storage containers not previously reviewed; with associated site improvements. Under separate review is a Site Plan Review application.
- B. **APPLICANT:** Dana Love
- C. **OWNER:** Dana & Denise Love
- D. **PROPERTY LOCATION:** The property is located at 416 Cascade; Legal Description: 3N10E25CD Tax Lot #4401 (See Attachment "B," Location Map)
- E. **ZONING:** The site is zoned General Commercial (C-2) and within the Exit 63 Interchange Area Management Plan (IAMP) overlay and Downtown Historic District overlay.
- F. **PROPERTY SIZE & USE:** The subject property is approximately 7,500 square feet in area. The property includes a commercial building (Promotion Wetsuits) and existing temporary food carts with Transient Merchant Licenses.
- G. **SURROUNDING ZONING & LAND USES:**
North: C-2, movie theater
South: C-2, city parking lot
East: C-2, US Post Office
West: C-2, office/retail and city parking lot
- H. **APPLICABLE HOOD RIVER MUNICIPAL CODE (HRMC) CRITERIA:**
1. HRMC 17.09.040 – Quasi-Judicial Actions
2. HRMC 17.14.110 – Review of New Construction
- I. **AGENCY COMMENTS:**

1. Joy Sears, Oregon State Historic Preservation Office: Comments attached (Attachment "C")

J. **NEIGHBORING PROPERTY OWNER COMMENTS:** Property owners within 250 feet of the subject parcel were notified of this request. No comments were submitted by neighboring property owners in response to the Landmarks hearing notice prior to issuance of the staff report.

K. **HISTORY:**

1. Promotion Building approved in Site Plan Review File No. 1998-37 & constructed after with a Certificate of Occupancy granted in December, 2002
2. Landmarks Review & Site Plan Review Applications submitted May 30, 2019
3. Additional application materials submitted August 1, September 9 & October 17, 2019
4. Applications deemed incomplete June 27, August 29, October 9 & November 14, 2019
5. File closed November 21, 2019
6. New File created November 21, 2019
7. Additional application materials submitted January 23, March 10 & April 28, 2020
8. Applications deemed complete May 26, 2020
9. Agency referrals mailed and e-mailed June 4, 2020
10. Notice of Landmarks Review Board hearing mailed June 30, 2020
11. Landmarks Review Board hearing scheduled July 21, 2020

L. **ATTACHMENTS:**

- Attachment "A.1" – Site Plans
- Attachment "A.2" – Perspective Drawings
- Attachment "A.3" – Detailed perspective drawings with legend
- Attachment "A.4" – Bathroom addition elevation drawing
- Attachment "A.5" – Landmark application narrative, excerpts
- Attachment "A.6" – Original Promotion Building South Elevation
- Attachment "B" – Location Map
- Attachment "C" – Email from Joy Sears, Oregon SHPO, November 26, 2019
- Attachment "D" – Photo of Southern façade Promotion Building, Google street view
- Attachment "E" – Example Food Cart Park Designs
- Attachment "F" – Design Guidelines for Local Historic District

II. **FINDINGS OF FACT:**

1. **HRMC 17.14.110 REVIEW OF NEW CONSTRUCTION:**

A. **Purpose.** The purpose of reviewing the exterior design of new construction is to ensure that new construction is compatible with the character of the district or designated historic landmark located on the same parcel.

FINDINGS: The findings and conclusion in this report address the purpose of ensuring the design of the proposed food cart park, building addition, and storage containers are

compatible with the City of Hood River Downtown Historic District.

- B. **Initiation.** The process for applying for new construction may be initiated by the property owner or authorized agent, upon submittal of a complete application.

FINDINGS: The subject application was submitted and signed by the property owner, Dana Love, consistent with this requirement.

- C. **New Construction.** Review is required for any new construction, which occurs on the same parcel as a designated historic landmark, or on any parcel in a designated district.

FINDINGS: The subject property, approximately 7,500 square feet in area, is located at the northeast corner of 5th and Cascade inside the City's Downtown Historic District. The eastern half of the site includes an existing commercial building, approved in 1998 (File No. 1998-37) as a Site Plan Review. The following year, an ordinance made clear that new construction in the City's Downtown Historic District is subject to review by the Landmarks Board (Ordinance No. 1774).

The proposed project includes three elements of new construction:

1. Construction of an addition to the existing building for a new restroom (approximately 60 square feet) and a service window on the west wall of the existing building;
2. Construction of food cart park to accommodate up to 3 food carts and a new food/beverage establishment inside the building that includes an outdoor seating area with pergola, patio with gas fire pit, sail shade structure, fence and other site amenities; and
3. Placement of 3 storage containers (8' x 40' each) which are currently on-site but not previously reviewed.

Pursuant to HRMC 17.14.110.C, Landmark Review is required for each of these elements of new construction.

For clarification, various food carts permitted with Transient Merchant Licenses have occupied the vacant western portion of the subject property for years. Food carts with Transient Merchant Licenses are permitted to operate on commercial properties in Hood River pursuant to HRMC 5.07 and with permission by the property owner. Transient Merchant Licenses are not subject to land use review under Title 17, they are valid for up to 6 months, and they prohibit tables or seating for patrons (HRMC 5.07).

The applicant now proposes to develop the vacant western half of the site with a seating area and restroom for food carts until such time that it is economically feasible to finish the larger building originally envisioned. A Site Plan Review is required for the proposal and, if approved, will not limit the time period for any individual food cart or the food cart park.

- D. **Procedure.** A request to construct a new structure shall be referred to the Landmarks Board who is the final review body unless an appeal is filed. The Landmarks Board shall conduct a quasi-judicial hearing in accordance with the requirements of Chapter 17.09 of the Hood River Municipal Code taking into consideration the review criteria and public testimony.

FINDINGS: The Landmarks Review Board is the review body for municipal code provisions

related to historic preservation, specifically applicable provisions in HRMC 17.14, Historic Preservation. The subject application for new construction located on a property in the City's Downtown Historic District and processed in accordance with the Quasi-judicial procedures in HRMC 17.09.040.

- E. **Application.** An application for new construction shall be prepared and filed with the Planning Department, using forms prescribed by the Planning Director. The Planning Director shall fix a date and time for a public hearing before the Landmarks Board.

FINDINGS: The application materials generally address the submittal requirements. A public hearing before the Landmarks Board is scheduled for Tuesday, July 21, 2020, consistent with these requirements.

- F. **Relationships to Other Planning Review.** Projects, which require a historic review, may also require other land use reviews. If other reviews are required, the review procedure may be handled concurrently.

FINDINGS: The subject proposed food cart park with seating area, conversion of existing space inside the building to a food/beverage establishment, addition to the building, and placement of storage containers are commercial uses that require a Site Plan Review application. The Site Plan Review application is under review as an Administrative Decision. A decision on the Site Plan Review application will be issued prior to September 23, 2020, pursuant to ORS 227.178.

- G. **Review Criteria.** In reviewing the request, the Landmarks Board shall consider the following criteria:

1. **The design of new construction is compatible with the design of the historic landmark(s) on the parcel or in the district, considering scale, style, height, and architectural detail and materials. The Landmarks Board shall use the Secretary of the Interior's "Standards for the Historic Preservation with Guidelines for Applying the Standards" and the Secretary of the Interior's "Standards for Treatment of Historic Properties" (Chart "A" - Secretary of Interior Standards).**

FINDINGS: Design of the proposed construction must be compatible with historic landmarks in the City's Downtown Historic District to maintain the historic integrity of the downtown. The City also adopted advisory Design Guidelines for the Hood River Local Downtown Historic District to assist the Landmarks Board consider the Secretary of Interior's Standards for Rehabilitation in the context of Hood River's historic district. In this case, there is no historic building on the property, but the existing building was designed to be compatible with the district. Thus, additions to the existing building should be compatible with the original design of the building. The proposed food cart park with seating amenities and proposed storage containers should be compatible both with the existing building and with landmarks in the district in general.

The applicant did not respond directly to the Secretary of Interior's Standards for Rehabilitation or the City's advisory Design Guidelines for the Hood River Local Downtown Historic District. The applicant provided a rendering of the building as it was

approved in 1998, a portion of which was constructed and exists today (Attachment A.6). The existing Promotion building is a two-story red brick building with art deco inspired concrete and decorative metal accents featured in its balconies and railings. The main entrance exhibits a recessed open-air vestibule allowing sunlight into the building through the glazed storefront while protecting visitors from the elements. The second level curved balcony above the entrance adds a slight relief, adding depth to the already recessed vestibule. This balcony helps identify the building's entrance.

The scale of the building is brought down to the pedestrian level with its small changes in brick profile and groupings of fenestration. The distinctive art deco inspired features include the empire shaped vertical concrete posts in the railing and nod to industry with the steel rounds and strong horizontal lines of the metal railing on the balcony and parapet on the rooftop level.

Overall, the building aesthetic has a modern design with a sensitivity in detail to the historic neighborhood it resides in.

The applicant provided perspective drawings of the proposed development (Attachments A.2 & A.3) and the following narrative regarding design of the project:

The new building was done in an architectural design to resemble turn of the century brick building on the sides facing the street. The new building was designed to be built in phases. The first phase built covers roughly 40% of the property. The remaining phases once completed will cover the balance of the property. It is our intention to finish the remaining phases as was originally approved. See photo attached – ProMotion Building as completed – West Elevation.

Until the later phases of construction become economically feasible, we are looking to make use of the vacant lot space, and are proposing an improved area for food carts. Currently the lot has food carts on site with no amenities. We plan to add the following...a bathroom, a pergola, sail shade canopy's, a fenced and paved patio, picnic benches, string lighting for evenings, and a gas fire pit. See documents submitted with application.

If we were adding the next phase to our existing building, it would be a simple matter of following the construction details as were originally approved. Instead we are proposing minimal improvements as a temporary use of the empty corner lot space.

.... we have looked at historical buildings in downtown Hood River and reviewed photo archives from the Hood River Museum for inspiration. From this, the following design features have been added or upgraded...

- 1>Diagonal Braces have been added to the pergola posts.*
- 2>River Rock Columns have been added to pergola posts.*
- 3>Exterior Trims have been added to the bathroom addition, proposed exterior doors, and service window area.*
- 4>The color scheme uses colors and tones to match the existing building and landscaping. These include red's, brown's, and green's.*
- 5>Brick Walkways match the existing building.*
- 6>Storage Containers at the rear of the building will be screened from view by food cart placement on the lower end of the lot.*
- 7>The Trash Containment Area design matches the pergola structure.*

(Excerpts from Applicant's narrative, Attachment A.5)

The proposed food cart park will include up to three food carts or trailers converted to food carts. The proposal also includes three storage containers. The food carts will likely change over time and are mobile. The Secretary of Interior Standards for Rehabilitation and the City's historic district design guidelines do not contemplate or address temporary, mobile structures such as the food trucks or storage containers. The food carts or trailers and the storage containers are not compatible with the appearance and character of the Downtown Historic District and not compatible with the design of the Promotion building. Thus, to mitigate the appearance of the carts and containers, fencing and landscaping can be required to screen them from adjacent public rights-of-way, especially if designed to complement the design features of the Promotion building. One approach is to create a perimeter wall that uses traditional materials, such as wood, masonry and metal work from the Promotion building and defines the edge of the food truck park and the predominant corner.

The following findings evaluate the scale, style, height, architectural detail, and materials of the proposal:

Scale: The existing Promotion building is approximately 40 feet wide (southern façade), 63 feet long (western façade) and 2 stories in height.

Buildings in the vicinity of the subject parcel include a mix of one and two-story buildings of various sizes.

The proposed development includes 3 food carts, with dimensions averaging about 9' in width by 20' in length, 3 storage containers (8' x 40' each), an addition with dimensions of approximately 7' x 8.5', and pergola (~18' x 24.5'). All of the proposed elements are approximately one story in height.

The proposed development appears to be compatible with the scale of the existing building on-site and with historic buildings in the surrounding area.

Style: As described by the applicant, the existing Promotion building is a portion of the originally proposed building with "*an architectural design to resemble turn of the century brick building on the sides facing the street.*" The southern façade and the southern half of the eastern façade are constructed in this style. These facades include brick construction with aluminum clad glazed storefront windows, a main entrance with a recessed vestibule and curved balcony above, art deco inspired concrete and decorative metal railing featured on the balcony and decorative brick accents above and below some of the windows. The building also features a flat roof shielded with a decorative parapet. The western half and 3rd story of the original plan were not constructed, thus only the southern façade and the eastern façade were finished as envisioned in the original plan. The northern half of the eastern façade is a blank stucco wall. The northern façade and western façade, facing the portion of the site proposed for the bathroom addition and food cart park, are virtually blank, window-less walls with wood siding painted to match the color of the brick.

Buildings in the vicinity of the subject parcel include a mix of architectural styles. Other

existing buildings in the vicinity include “new” construction (Hood River Cinemas and the brick commercial building at 504-510 Cascade). Historic buildings in the vicinity include the Post Office (Moderne style) immediately east, cannery buildings to the north and retail buildings to the south. The building kiddy-corner to the southwest at 104 5th Street (Solstice catering kitchen, Holistic Massage) is not included on the inventory of historic buildings in the downtown district. Buildings in the vicinity include a mix of styles and materials, including stucco, wood, concrete, and brick.

The applicant provided an elevation drawing of the proposed bathroom addition (Attachment A.4) that indicates the exterior materials will be predominately wood with a metal shed roof. Given the general “turn of the century brick” style of the Promotion building, the proposed addition does not appear to be designed to be compatible with the general style of the building.

The pergola, food carts, and storage containers introduce contemporary elements to the site, including metal siding (storage containers), river rock (pergola posts), horizontal wood slat privacy wall, gas fire pit, sail shades, and string lighting. The perspective drawings (depict) decorative fencing around the seating area and pergola. The proposed fencing around the seating area includes a mix of wood and metal – it does not include any brick, concrete, or type of decorative metal railing elements found on the building.

Food trucks and storage containers contrast with the traditional built environment of the Downtown Historic District. The Landmarks Board will need to consider whether the style of the proposed building addition and food cart park with a fence around the pergola and seating, render the proposal as a whole compatible with the style of the Promotion building and historic district. One approach is to require that the restroom addition is designed to blend with the style and architecture of the Promotion building and that a perimeter wall is designed to incorporate materials used on the Promotion building (masonry, metal railing, wood) to screen the food carts and storage containers, and blend the food truck park with the Promotion building and historic district traditional built environment. This can be achieved with careful design that avoids introducing a new style or too many new design elements. The design should also incorporate landscaping and trees along the 5th Street frontage to help soften the appearance of the food carts.

Height: The existing Promotion building is two stories in height. Surrounding buildings are one or two stories in height. The proposed new construction is approximately one story in height and appears to be compatible in height with existing development in the district.

Architectural Detail: Architectural details of the Promotion building include large, vertical aluminum clad glazed storefront retail windows, transom windows, a main entrance with a recessed vestibule and curved balcony above, art deco inspired concrete and decorative metal accents featured on the parapet, balconies, and railings, and decorative brick accents above and below some of the windows.

Architectural details of the proposed addition include a metal, shed roof and horizontal wood slat privacy wall. For the pergola, details include river rock accents on the post

columns, diagonal braces on the posts, and a metal roof. The perspective drawings (Attachments A.2 & A.3), depict decorative fencing, sail shades, and string lighting. The fence is proposed to be constructed of a “combination of wood and metal.”

The storage containers have no architectural features and the applicant argues they will be screened by food carts. The food carts also have no architectural features and will likely rotate.

The Landmarks Board will need to consider whether the architectural features of the proposed building addition and food cart park are compatible the architectural features of the Promotion building and historic district. If not, the Landmarks Board can consider conditions of approval to require a new design that incorporates features such as linear masonry columns, art deco inspired concrete, and metal railing decorative accents.

Materials: Exterior materials on the Promotion building include brick as the primary material with concrete accents, metal railing, aluminum clad windows, and wood siding painted to match the brick. The painted wood siding on the west and north façades were intended to be short-term until the full extent of the original building plans was built-out. The building has a flat roof with no visible roof material.

Proposed exterior materials for the addition include wood siding and metal roof with painted green trim. The applicant submitted a photo of a distressed metal roof as an example. Materials for the pergola include a wood structure with river rock accents and metal roof (distressed metal or green metal). The fire pit appears to be concrete. The proposed ground surfaces appear to include gravel, pavers and brick walkways. The fence is proposed to be a combination of wood and metal. Other materials include green canvas for the sail shades. The storage containers are metal, painted to match the brick building.

The Landmarks Board will need to consider whether the proposed exterior materials for the proposed building addition and food cart park are compatible the exterior materials of the Promotion building and nearby buildings in the historic district. If not, the Landmarks Board can consider conditions of approval to avoid contrasting or conflicting materials such as river rock and require use of exterior materials from the Promotion building, such as masonry and metal (bronze?), to ensure the project as a whole is designed appropriately for the historic district setting.

In sum, food carts and storage containers are mobile, somewhat temporary structures that are not contemplated in or compatible with the Secretary of Interior’s Standards for Rehabilitation or the city’s design guidelines for the Downtown Historic District. Because the food carts and storage containers may be located on the site for an indefinite period of time if the Site Plan Review is approved, staff recommends some type of screening or façade structure to screen the use from public rights-of-way and better blend the use into the site and Historic District. Using decorative screen walls that incorporate building materials from the Promotion building can maintain and define the site. They can also be used to screen (not conceal) utilitarian elements of the outdoor space that may appear unsightly or contrary to the traditional built environment of historic downtown. The design of the food cart park introduces several

new materials and styles. The use could be made to be more compatible with the Promotion building if it incorporated brick, decorative concrete and metal railing into the fence, expanded the fence and incorporated trees and landscaping to screen the entire west side, and eliminated materials that are incompatible such as river rock, distressed metal, and green wood trim.

The proposed addition design does not fully blend with the existing building because it adds a new roof form and new exterior materials and colors. The addition design could be revised to maintain the architectural style of the Promotion building.

The Landmarks Board will need to determine whether the scale, style, design, height, architectural detail, and materials of proposed hotel are consistent with this criterion.

- 2. The location and orientation of the new construction on the parcel is consistent with the typical location and orientation of similar structures on the parcel or within the district considering setbacks, distances between structures, location of entrances, and similar citing considerations. The Landmarks Board shall use the Secretary of the Interior's "Standards for the Historic Preservation with Guidelines for Applying the Standards," and the Secretary of the Interior's "Standards for Treatment of Historic Properties" (Chart "A" - Secretary of Interior Standards).**

FINDINGS: The site includes the existing Promotion building. As previously described, the original building plan included 3 stories and a larger building intended to occupy the full site. The existing building is 2 stories and occupies most of the eastern half of the site. The western half of the site is vacant with one to three food trucks located there under Transient Merchant Licenses issued by the city. The licenses are valid for up to 6 months and prohibit tables or seating for patrons (HRMC 5.07).

The location of the new construction is on the north and west sides of the subject property: the storage containers are proposed on the north side of the existing building (in current location which has not been previously reviewed); the building addition and food cart park are proposed on the west side of the existing building. The food trucks are proposed to be located on the north half of the vacant area and, as proposed, will not be screened by fencing as depicted on Attachment A.1.

Because food trucks and storage containers are inherently contemporary, utilitarian, mobile features, careful consideration for the location, and orientation of these features is recommended. Siting these features in locations set back from the public right-of-way in a manner that they can be screened from the street with a perimeter wall or fence will help harmonize the food truck park use with the existing building and create a sense of enclosure. Outdoor dining is not uncommon in Hood River's historic downtown, however siting of the mobile elements and use of a decorative screen wall or fence that incorporates building materials and design from the existing Promotion building will maintain the pedestrian zone and harmonize the proposed use with the brick and mortar architecture.

There is no similar development in the Downtown Historic District by which to compare

consistency of location and orientation. Siting is one important way to de-emphasize the elements not compatible with the Secretary's guidelines (food trucks & storage containers) and emphasize a perimeter screen that is compatible with the existing building and pedestrian-scale storefronts in the downtown.

The Secretary of Interior's Standards for Rehabilitation are more specifically addressed below followed by the Design Guidelines for the Hood River Local Downtown Historic District:

3. Chart "A" – Secretary of Interior Standards, referred to above include the following standards:

- (1) A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.**

FINDINGS: At the time the Promotion building was approved, there was an existing building on-site that was demolished. The prior building was a warehouse-style building used for selling farm equipment. The existing Promotion building is designed for retail with retail display windows on the ground level. Office space occupies the 2nd story. The original plan for the Promotion building envisioned 3rd story residential units.

The proposal envisions incorporating a new use – food & beverage. A portion of the retail space is proposed to change to a food/beverage establishment and use of the western vacant half of the property is proposed for food & beverage. Use of the area directly north of the building is proposed to be used for storage (storage containers). The containers, which have not been previously permitted, are used for storage of wetsuits and other retail inventory for Promotion Wetsuits.

The proposed new food & beverage use and outdoor dining in general is common in the Downtown Historic District, however the particular type of food & beverage use – a food cart park – is not. This would be the first such development in the Downtown Historic District or Hood River at large. The development would be primarily adjacent to the existing Promotion building and, thus, would not change the defining architectural characteristics of the building. It would change the character of the site.

The proposed addition and service window would change the character of a portion of the retail space on the ground level, but no changes are proposed to the street-facing south façade or entrance to the building.

Given that the western portion of the subject property is vacant, some improvement of the property will be welcome. Ideally, the original building will be built-out as planned. In lieu of that, the proposed food cart park may be an improvement over current conditions, particularly if it is designed to be compatible with the style and design of the Promotion building and Downtown Historic District by using siting of the food trucks and designing a screening wall to de-emphasize the

food trucks and storage containers and create a perimeter structure that extends the features of the Promotion building to the corner and creates a street frontage that harmonizes with the existing building and greater historic district. While the food carts or trailers converted to food carts and storage containers are not compatible with the appearance and character of the Downtown Historic District and not compatible with the design of the Promotion building, screening including fencing and landscaping can screen the carts and containers from adjacent public rights-of-way. With careful design, the proposed new use of the site may result in minimal change to the defining characteristics of the site and district to be consistent with Standard 1.

(2) The historic character of a property shall be retained and preserved. The removal of historic material or alteration of features and spaces that characterize a property shall be avoided.

FINDINGS: Because the property was redeveloped in the early 2000s, there is no historic character of the property. The Promotion building was designed, however, to be compatible with the Downtown Historic District and any new uses or development of the site should maintain that compatible character. Design features on the Promotion building that are inspired by the Historic District are not proposed to be altered or removed. The proposal is not in conflict with this standard.

(3) Each property shall be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or architectural elements from other buildings, shall not be undertaken.

FINDINGS: The food carts and storage containers themselves introduce features conjectural to the Historic District. While the proposal is not likely to create a false sense of historical development, it does introduce a contemporary use. As described elsewhere in these findings, carefully designed screening and siting is required to ensure consistency with this standard.

(4) Most properties change over time; those changes that have acquired historic significance in their own right shall be retained and preserved.

FINDINGS: Staff is not aware of any structures or development on the property that is eligible for historic significance given the recent redevelopment of the site (less than 50 years ago).

(5) Distinctive features, finishes and construction techniques or examples of craftsmanship that characterize a historic property shall be preserved.

FINDINGS: As described, the Promotion building includes distinctive features that, while not historic, are designed as compatible with the Downtown Historic District. These include art deco inspired concrete and decorative metal railing accents featured on the balconies and parapet, decorative brick banding accents above and below some of the windows, a main entrance with a recessed vestibule, glazed

storefront windows, and a curved balcony above the entrance. No changes to these features are proposed.

- (6) Deteriorated features shall be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature shall match the old in design, color, texture, and other visual qualities, and where possible, materials. Replacement of missing features shall be substantiated by documentary, physical, or pictorial evidence.**

FINDINGS: There are no deteriorated features. This standard is not applicable.

- (7) Chemical or physical treatments, such as sand blasting that cause damage to historic material shall not be used. The surface cleaning of structures, if appropriate, shall be undertaken using the gentlest means possible.**

FINDINGS: No historic material will be altered. This standard is not applicable.

- (8) Significant archaeological resources affected by a project or development shall be protected and preserved according to Oregon Revised Statute ORS 358.905. If such resources must be disturbed, mitigation measures shall be undertaken.**

FINDINGS: There are no known archaeological resources on the site, however a condition of approval is recommended to alert the applicant and owner that state law governs the protection of archeological objects and sites.

- (9) New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.**

FINDINGS: As described a small addition and related new construction is proposed on a property located within the Downtown Historic District. The property does not include any historic structures, however the existing building on the subject property was designed to be compatible with the Downtown Historic District. For this application, staff interprets the standard to require that the proposed addition to the building shall be compatible with the design of the building, and the proposed storage containers and proposed food cart park with seating amenities shall be compatible both with the existing building and with landmarks in the Downtown Historic District.

The proposed restroom addition is small, however its design does not include any features or highlighted exterior materials (brick with concrete & steel accents) from the existing building.

The related new construction – storage containers and food cart park – also is not clearly designed to blend with the existing building. Although the storage containers and food carts may be intended as temporary, there is no time limit for

the proposed uses and the proposal requires Site Plan Review, including connection of the carts to city utilities. Taking into account the uses may be located on the site for an extended period of time, a more permanent façade structure, fencing, or landscaping – or combination thereof – will be required to screen the carts and containers from public rights-of-way and ensure the proposed uses are compatible with the characteristics of the Downtown Historic District. In addition, materials used on a screening fence or other structures should incorporate materials and design features from the Promotion building.

The Oregon SHPO was unable to provide comments specific to this application, however SHPO staff provided the names of three preservation consultants to the applicant to assist with design of the proposed use (Attachment C). It is not clear whether the applicant pursued this guidance. In addition, SHPO staff provided city staff information about a similar proposal in the City of Bend in which the Bend Landmarks Commission took the approach that the food trucks could not be made compatible with historic buildings and, thus, screening the use from the adjacent public right-of-way was required.

(10) New additions and adjacent or related construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

FINDINGS: The applicant intends that the food carts and storage containers are temporary “*until the later phases of construction become economically feasible,*” to develop the rest of the originally envisioned building. Although the timeframe for construction of the remainder of the building is unknown, the storage containers and food trucks have no foundation and can be removed without altering the Promotion building. It will also be possible to remove the foundation anchors for the pergola, fence and other site amenities in the future without altering the defining features of the Promotion building.

The original design of the building did not include a small addition on the west side for a restroom, a service window, or service door. Because the west wall is constructed of wood, not brick, and the west wall does not have any defining design features of the building, it appears this portion of the proposed development also could be removed in the future without affecting the integrity of the building, however staff recommends that the addition is designed to blend with the architectural style of the building.

Design Guidelines for the Hood River Local Downtown Historic District

These design guidelines (Attachment “E”) are advisory with the purpose of providing rehabilitation and new construction guidelines to owners of building in the downtown historic district. The guidelines also provide assistance to the City and the Landmarks Board for reviews of alterations and new construction in the District. The Design Guidelines are designed to encourage Hood River residents and owners of historic properties or properties in the District to appreciate and preserve the local architecture and history which helps define the unique character of the community.

The Design Guidelines includes Guidelines for Additions and New Construction that the Landmarks Board can use to help interpret consistency of the proposal with HRMC 17.14.110.G.

New Additions Guidelines

NEW ADDITIONS (excerpts)

...changing needs may increase the desire for expansion of these historic structures.

Guidelines:

- A. Preserve significant historic materials and features.
- B. Avoid attaching additions on primary (or street) elevations.
- C. Design the addition to be subordinate to the historic building.
- E. Consider setting the connector back from the historic building's wall plan so the form of the historic building can be distinguished from the new addition.
- H. Design additions so that they are smaller in scale so as not to overpower the historic building.

FINDING: Due to changing needs, instead of building out the Promotion building as originally envisioned in 1998, the applicant proposes to convert a portion of the building to a food/beverage establishment which includes adding a small restroom addition on the west side. As discussed above, the proposed addition is for a contemporary building designed to be compatible with the Downtown Historic District. Staff interprets the applicability of these guidelines to suggest that additions to the Promotion building should fit with the design of the building and not add features discordant with the building or historic district. The proposed addition is set back from the south-facing primary street façade of the building, much smaller than the existing building, and will not alter the distinctive design features of the primary façade of the building. The addition has not been designed as an extension of the brick building with art deco inspired features, however, and may appear more of an appendage to the building than an extension of it. It also introduces a new roof form and material. Given the size and low height of the addition, the impact of the proposed addition will be mitigated if it is screened by carefully designed fencing and vegetation as viewed from public rights-of-way. The Landmarks Board may want to consider requiring the addition to be designed to be an extension of the design of the Promotion building, using exterior materials and design features of the building façade.

New Construction Guidelines (excerpts)

These design guidelines emphasize compatibility, context, and design elements rather than styles, which allow for a broad and flexible approach to new construction within an historic district.

STREETSCAPE and SETBACKS (excerpts)

The majority of the historic buildings in downtown Hood River are flush with the sidewalk, abut one another....and have recessed entries. Any new construction should address the street in a manner consistent with neighboring structures and overall street form and character.

FINDING: Guidelines in the document address traditional commercial setbacks and design features such as recessed entries but not the pedestrian-scale development such as food cart parks. Staff recommends incorporation of a perimeter wall or fence flush with the sidewalk that extends the style of the Promotion building along the street frontages to be consistent with the overall street form of the historic district. The design could incorporate a recessed entry at the corner or other location to be compatible with pedestrian scale storefronts.

BUILDING HEIGHT (excerpts)

Generally, the historic resources in the District are one and two-story buildings with the exception of a few three and four-story structures.... Three and four-story buildings are anchor buildings, located on corner lots.

Guidelines:

- A. Maintain the range in building heights from one to two stories; this variation creates a visually interesting street as well as retaining the character of the historic streetscape.

FINDING: The existing Promotion building is two stories in height. No new buildings are proposed with the food cart park, however taken as a whole, the addition, food carts, pergola, sail shades, and storage containers will not exceed approximately one story in height. Although located on a corner lot, the proposed lower-profile development is within the range of typical building heights and will add variation to the streetscape.

BUILDING WIDTH (excerpts)

Building width is one of the most predominate visual qualities in historic downtown. Building widths in downtown Hood River...vary from 27 ft. to 100 ft. wide.

Guidelines:

- A. Maintain the height-to-width ratio with other buildings in the immediate area.

FINDING: The proposed addition will be much smaller than the building and not consistent with the height-to-width ratio of historic commercial buildings. It will be screened, however, by the proposed site amenities associated with the proposed food cart park.

ROOF FORMS (excerpts)

The roofs of the commercial buildings in the District are generally flat or sloped and hidden by parapets or false fronts. Parapets add character to a building and are often stepped and embellished with cornices, special facing material, or decorative details. Parapets are a prominent feature in Hood River's downtown.

Guidelines:

- A. Hide the roof plane from view by the use of parapets.
- B. Encourage the use of decorative details on the parapet using examples from surrounding buildings.
- D. Use similar types of roofing material to others in the District on roofs that are visible from adjacent streets or buildings.

FINDING: The applicant's narrative does not address the Downtown Historic District design

guidelines. The existing building has a flat roof with a parapet embellished with art deco inspired steel and concrete decorative features. The proposed addition will have a metal shed roof. No parapet is proposed. The proposed roof material and form are not typical of other roof forms on historic buildings in the District. **Staff encourages the Landmarks Board to consider whether to require that the addition to be designed with a roof consistent with the roof of the Promotion building or whether to allow the proposed roof form if screened by fences and other features.**

The proposed pergola is also designed with a shed roof. The Landmarks Board will need to consider whether the proposed shed roof for the pergola is compatible with the site and historic district.

Sail shades are not contemplated in these design guidelines. The Landmarks Board will need to consider whether they are compatible with the historic district.

OPENINGS: Window Types and Proportions

The buildings in downtown Hood River were designed to house a variety of enterprises. These businesses often had central recessed entries that provided more window display space and shelter from the elements along with emphasizing the entrance from the sidewalk. Large display windows usually flanked the entries and low wooden, tile, or masonry bulkheads were built below the storefront windows. A band of horizontal transom windows were generally built above the storefront windows and entries. Historically the entries were usually composed of single or paired doors made with large glass panes with wood surrounds.

Guidelines:

- A. Recess primary entries and orient to the street rather than the side or rear.**
- B. Use large panes of glass in the entry doors (paired or single). The use of solid doors is not recommended on the primary or street facades.**
- C. Incorporate transom windows above entries or expose existing transoms.**
- D. Use large, clear plate glass in display storefront windows on street level with transom windows above and bulkhead below.**
- E. Generally use double-hung windows either paired or singly for the upper floor windows.**
- F. Maintain a clear visual division between the lower and upper stories by a change in material, surface texture, architectural detail, or use of awnings and/or canopies to define the horizontal division.**
- G. Maintain the rhythm of the window pattern and the ratio of the solid surface area to window area.**

FINDING: The applicant's narrative does not address the Downtown Historic District design guidelines. No windows are proposed on the addition, and no changes to the existing large retail and transom windows on the primary (south) façade are proposed. A new service window is proposed on the west side of the existing building. While the proposed service window does not fit these guidelines, it will likely be mostly screened from the abutting streets by the other proposed site amenities.

MATERIALS

The building materials used in the construction of the historic buildings downtown was predominately brick, hollow clay tile, and concrete. The sense of cohesiveness and continuity of the District derives in part from the consistent use of these building materials. The earliest buildings were constructed of brick; the later buildings are almost all concrete or hollow clay tiles. Common trim materials used historically include wood, brick, sheet metal, and concrete. The window sashes were constructed of wood on the earlier structures.

Guidelines:

- A. Use materials in rehabilitation projects and new construction that are compatible with the historic downtown in quality, color, texture, finish, and dimension.
- B. Encourage the use of wood windows.

Avoid (These represent different building materials not generally found in Hood River)

- A. Vinyl siding or trim, vinyl windows, vinyl awnings
- B. Aluminum siding
- C. Wood siding consistent with residential construction
- D. Rustic wood shakes, barn wood
- E. Corrugated metal
- F. Corrugated fiberglass
- G. Imitation rock, wood, stone, or brick veneers
- H. Metalized reflective or “smoked” glass
- I. Glass block
- J. Wood shingle façade coverings or canopies

FINDING: The contemporary Promotion building was designed to be compatible with buildings in the Downtown Historic District. It features brick construction, with concrete and steel decorative features. The building was not fully constructed as originally envisioned and includes wood siding on the west and north walls where future construction was anticipated.

The applicant’s narrative does not address the Downtown Historic District design guidelines. The design of proposed restroom addition to the Promotion building uses wood siding and a metal roof, both of which are atypical of Hood River’s historic buildings and discouraged by the guidelines. Staff encourages the Landmarks Board to consider whether to require that the addition to be designed with materials consistent with the primary materials of the Promotion building or whether to allow the proposed wood siding and metal roof if screened from abutting public streets by fences and other features.

The proposed storage containers are metal. While sheet metal is considered an acceptable trim material, it is recommended to avoid the use of corrugated metal and sheet metal is not anticipated as a primary exterior material. The placement of the containers north of or behind the existing building screens them from Cascade Avenue. Again, staff encourages the **Landmarks Board to consider whether to allow the placement of the storage containers if they can be adequately and appropriately screened from 5th Street.**

These guidelines do not contemplate materials for moveable elements such as food carts. Although intended to be temporary and changeable, if the use receives Site Plan Review

approval, there will be no time limit conditioned for the carts and associated site amenities. Taking into account the carts may be located on the site for an indefinite period of time, and potentially rotated with other food carts, some type of screening or more permanent façade structure is needed to screen them from view of adjacent public streets. As such, staff recommends that the proposed fencing for the outdoor seating area is extended to screen the full frontage of the site along 5th Street and that the fencing is designed to incorporate materials used on the Promotion building – brick, steel, and concrete.

The proposed pergola is designed to be constructed with wood and river rock accents on the post columns. The proposed roof material is distressed (rusted) metal or green metal. River rock, distressed metal, and green metal roofing are not typical of Hood River’s historic district and staff recommends **a condition of approval is included to require the use of a dark metal for the roof, prohibit the use of river rock, and to significantly limit the use of distressed metal.**

AWNINGS & CANOPIES (excerpts)

Awnings and canopies provide protection from the elements and create a sense of enclosure to the street. Historic photos show that awnings were plain in design, fit within the window opening or are outside the opening, retractable, and usually striped or solid in color with scalloped or straight edges. Awnings were mainly used on the north side of the buildings in Hood River rather than the south side.

Guidelines:

- B. Fit awnings and canopies (if possible) within window bays and do not overlap multiple window openings, unless historic photos indicate otherwise.
- C. Awnings should not detract or conceal the architectural details or features of the building.
- D. Retractable fabric awnings were used historically and are recommended as they can be adjusted to varying light and weather conditions.
- E. Use an awning slope of no more than 45 degrees.
- F. Choose awning colors that are compatible with the color of the building. Avoid brightly colored or “busy” patterns.
- G. Use flat, horizontal metal canopies suspended by chains or rods, if original.

FINDING: The applicant’s narrative does not address the Downtown Historic District design guidelines. The plans include several sail shades forming a sort of tent over a portion of the proposed outdoor seating area. Green canvas sail shades are proposed. As depicted in the perspective drawings (Attachments A.2 & A.3), it also appears that a canopy may be proposed above the proposed service window.

The proposed sail shades are a contemporary feature and different from the canopies and awnings found on historic buildings. They do provide a sense of enclosure and some protection from the elements. The grouping of several sail shades to form a tent-like structure may detract from the building, however they are placed on the west side of the building which lacks architectural details or features of the building. If allowed, staff **recommends a condition of approval to require that the sail shades are solid colors that blend with or complement the color of the building.**

It is not clear what materials are proposed for the canopy over the proposed service window. **If a canopy is proposed on the west side of the building above the service window and door, staff recommends it is either a flat, horizontal metal canopy suspended by chains or rods or that it is a solid canvas awning (no pattern) with a color that complements the brick-red color of the building.**

SIGNAGE

The Design Guidelines for the Hood River Local Downtown Historic District include guidelines for new signage:

- Relate signs in placement and size to other building elements.
- Avoid obscuring windows and decorative details with signage.
- Complement the sign material, style, and color with the building façade.
- Keep night lighting of signage subtle and in keeping with the architectural style.
- Use of plastic faced signs are not recommended.
- Encourage the use of signs as identification not advertising.

FINDINGS: The perspective drawings indicate a new sign is proposed on the west side of the proposed fence, facing 5th Street. No other details were provided, however a photo of an example laser cut metal sign was submitted. If located on the fence, the new signage will not obscure windows or decorative exterior building features. **A condition of approval is included to alert the applicant to the requirement to obtain a sign permit for any new signs.**

COLOR

The Design Guidelines for the Hood River Local Downtown Historic District include recommendations related to color including:

- Harmonize and relate colors to the building material such as brick or wood.
- Blend the color choice with buildings in the same block.
- Choose a color that is similar to the original brick color on buildings that have been previously painted.
- Choose a maximum of three colors to highlight any building façade.
- Avoid using intense hues.
- Avoid colors that highly contrast or overly accent decorative details on the buildings.

FINDINGS: The Promotion building features brick construction, with unpainted smooth concrete and decorative bronze or steel metal features. The building was not fully constructed as originally envisioned and includes wood siding on the west and north walls painted to match the brick color. The proposed storage containers (already on-site) are painted to match the brick color.

The applicant's narrative does not address the Downtown Historic District design guidelines or provide details about colors other than in the narrative (Attachment A.5): *"The color scheme uses colors and tones to match the existing building and landscaping. These include red's, brown's, and green's."* Additional narrative indicate the sail shades are proposed to be green and green metal is proposed for the pergola roof and entrance gate roof.

While it appears that the proposed colors are generally intended to blend with the existing

building, some details are lacking. It is not clear whether the wood siding on the addition and the wood for the pergola structure are proposed to be painted. Also, fence colors are not specified. In addition, the applicant intends to rotate food carts, leaving open the possibility for any colors and color hues on those elements.

A condition of approval is recommended to require that the exterior colors for the proposed restroom addition, storage containers, fence, pergola structure and roof, and sail shades shall be selected to match or complement the color of the brick, steel and concrete on the Promotion building. Use of intense hues or colors that highly contrast with the colors on the Promotion building shall be prohibited.

H. Conditions of Approval. In approving applications for new construction, the Landmarks Board may attach conditions that are appropriate for the preservation of the historic or architectural integrity of the historic landmark/district. All conditions must relate to review criteria.

FINDINGS: Conditions of approval are recommended as noted within these findings and as listed below in order to ensure the proposal is consistent with the review criteria.

I. Decision. All decisions by the Landmarks Board under this section to approve, approve with conditions, or deny construction shall be supported by written findings and shall be forwarded to the property owner within seven (7) days of the decision.

FINDINGS: Recommended findings and conditions included in this report to assist the Landmarks Review Board review of the application. As noted below, staff recommends the Landmarks Review Board approve the proposed new construction with conditions. A copy of the final decision will be provided to the applicant and property owner within seven days of the Notice of Decision.

III. CONCLUSIONS: The City's historic preservation ordinance requires review of any new construction which occurs on a parcel located within the City's Downtown Historic District. The proposed food cart park, building addition, and storage containers are located on a site at the corner of 5th and Cascade which is in the City's Downtown Historic District. Thus, review of the new construction for compatibility with the historic district is required.

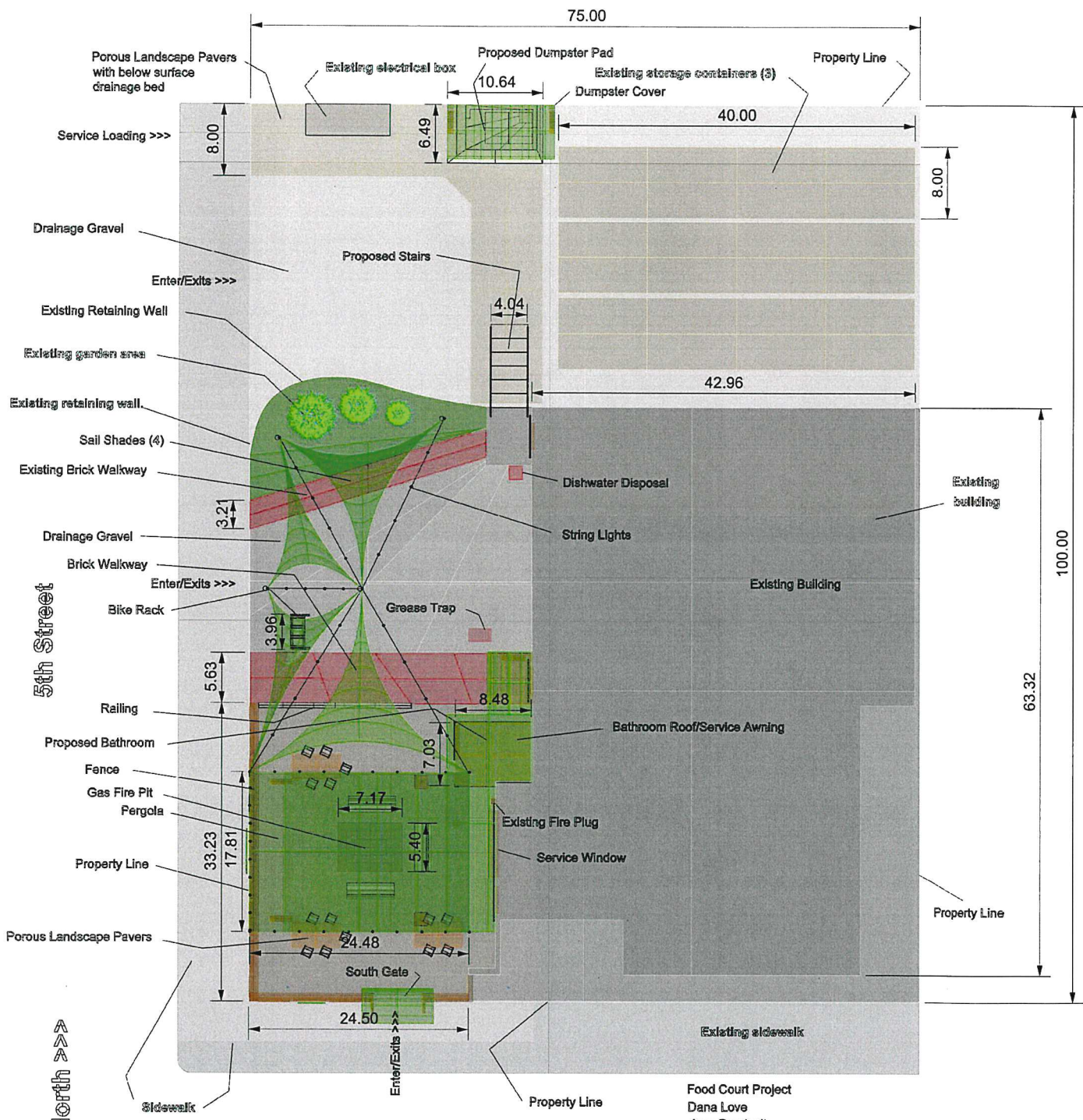
Although the applicant did not specifically address the Secretary of Interior's Standards for Rehabilitation or the City's advisory Downtown Historic District design guidelines, the perspective drawings and a narrative provide information to evaluate the proposal for compatibility with the district.

The subject property is partially developed with a building designed to be compatible with historic buildings in the district. The building was originally envisioned to include additional phases to build-out the entire site. The applicant now proposes to develop the vacant western half of the site with a food cart park until such time it is economically feasible to finish the building. A Site Plan Review is required for the proposal and, if approved, will not limit the time period for any individual food cart or the site amenities. Taking into account that the small addition, food cart park, and storage containers may be located on the site for an indefinite period of time, staff recommends that some type of more permanent fence or façade structure

and landscaping is provided to screen the proposed development from adjacent public streets. In addition, to ensure the proposed development is compatible with the historic district, the fence or other screening shall be designed to complement the Promotion building, incorporating materials, colors, and decorative features on the building.

- IV. RECOMMENDATION:** Based on the above findings of fact drafted in this report, and if the Landmarks Review Board makes findings of compliance with the historic preservation criteria, **staff recommends approval** of the Landmarks Review application subject to conditions of approval.
- V. DRAFT CONDITIONS OF APPROVAL:** Based on the above Findings of Fact and Conclusions of Law, the preliminary plans and all representations and statements made by the applicant, **staff recommends approval** of the Landmark Review request for construction of a small restroom addition (approximately 60 square feet), service window, and service door on the west side of the existing Promotion building, placement of three storage containers on the northeast portion of the site, and construction of a food cart park as described herein, **subject to the following conditions of approval:**
1. All new site disturbance and construction is subject to compliance with state law governs the protection of archeological objects and sites. All representatives working on the project should be aware of ORS 358.905-358.961 that governs archeological objects and sites - <http://www.oregonlaws.org/ors/358.905>, Permit and Conditions for Excavation or Removal of Archaeological or Historical Materials (ORS 390.235) - <https://www.oregonlaws.org/ors/390.235>, and the Administrative Rules for Archaeological Permits for Public and Private Lands (OAR 736-051-0000 through 0090) - <https://secure.sos.state.or.us/oard/displayChapterRules.action?selectedChapter=169>.
 2. This decision does not authorize any exterior alterations to the southern façade of the Promotion building. Any proposed exterior alterations to the southern façade of the building require a new Landmarks application and approval by the Landmarks Board.
 3. The restroom addition on the west side of the building shall be designed to incorporate exterior materials and design features of the southern façade of the Promotion building including a flat roof. The design shall avoid the use of green trim or other bright colors that highly contrast from the colors used on the Promotion building.
 4. The proposed service window on the west side of the building shall avoid the use of green trim or other features that clash with the design, features, materials, or color of the existing Promotion building.
 5. The design of the pergola shall incorporate masonry columns and metal accents found in the existing storefront to improve consistency of design with the existing Promotion building. Other exterior materials found on the existing Promotion building can be incorporated into the design of the pergola. The use of river rock shall be prohibited. A dark metal roof is permitted for the pergola, however a distressed metal roof is prohibited.

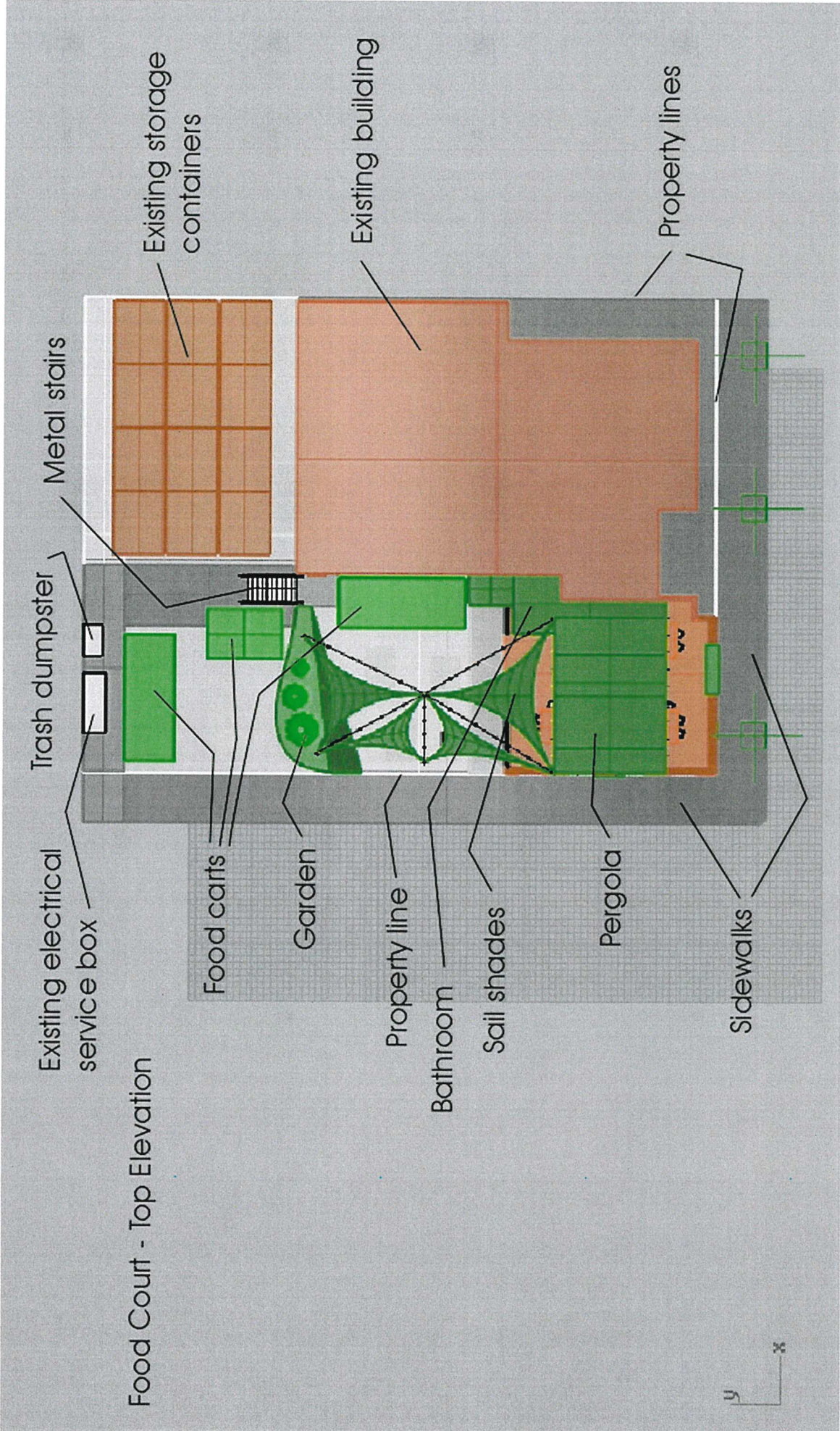
6. The proposed food cart park shall be designed to incorporate a perimeter wall or fence constructed of brick or concrete masonry units to match the Promotion building, and metal or wood accents that extends the style of the southern façade of the Promotion building along the south and west property boundaries and serves to screen the proposed food trucks and storage containers from adjacent public rights-of-way. Use of river rock shall be prohibited. Distressed metal may be used only as an accent treatment.
7. The proposed food truck pads shall be set back at least 5 feet from the perimeter wall or fence to allow for landscaping and circulation.
8. The exterior colors for the proposed restroom addition, storage containers, fence, pergola structure and roof, and sail shades shall be selected to match or complement the colors of the brick, metal windows and railing, and concrete on the Promotion building. Use of intense hues or colors that highly contrast with the colors on the Promotion building shall be prohibited.
9. Placement of three storage containers (8' x 40' each) directly north of the existing Promotion building is permitted if the containers are painted to match the color of the unpainted brick on the Promotion building and are screened by a fence and landscaping along the west perimeter of the property.
10. A sail shade canopy is permitted and shall incorporate metal matching the Promotion building in the structure and solid color shades that complement the colors found on the Promotion building. Use of intense hues or colors that highly contrast with the colors on the Promotion building shall be prohibited.
11. If a canopy is proposed on the west side of the building above the service window and door, it shall be either a flat, horizontal metal canopy suspended by chains or rods or a solid canvas awning (no pattern) with a color that complements the brick-red color of the building.
12. Perimeter screening along the southern and western street frontages shall incorporate landscaping to be reviewed and approved in association with the Site Plan Review application for the proposed food cart park.

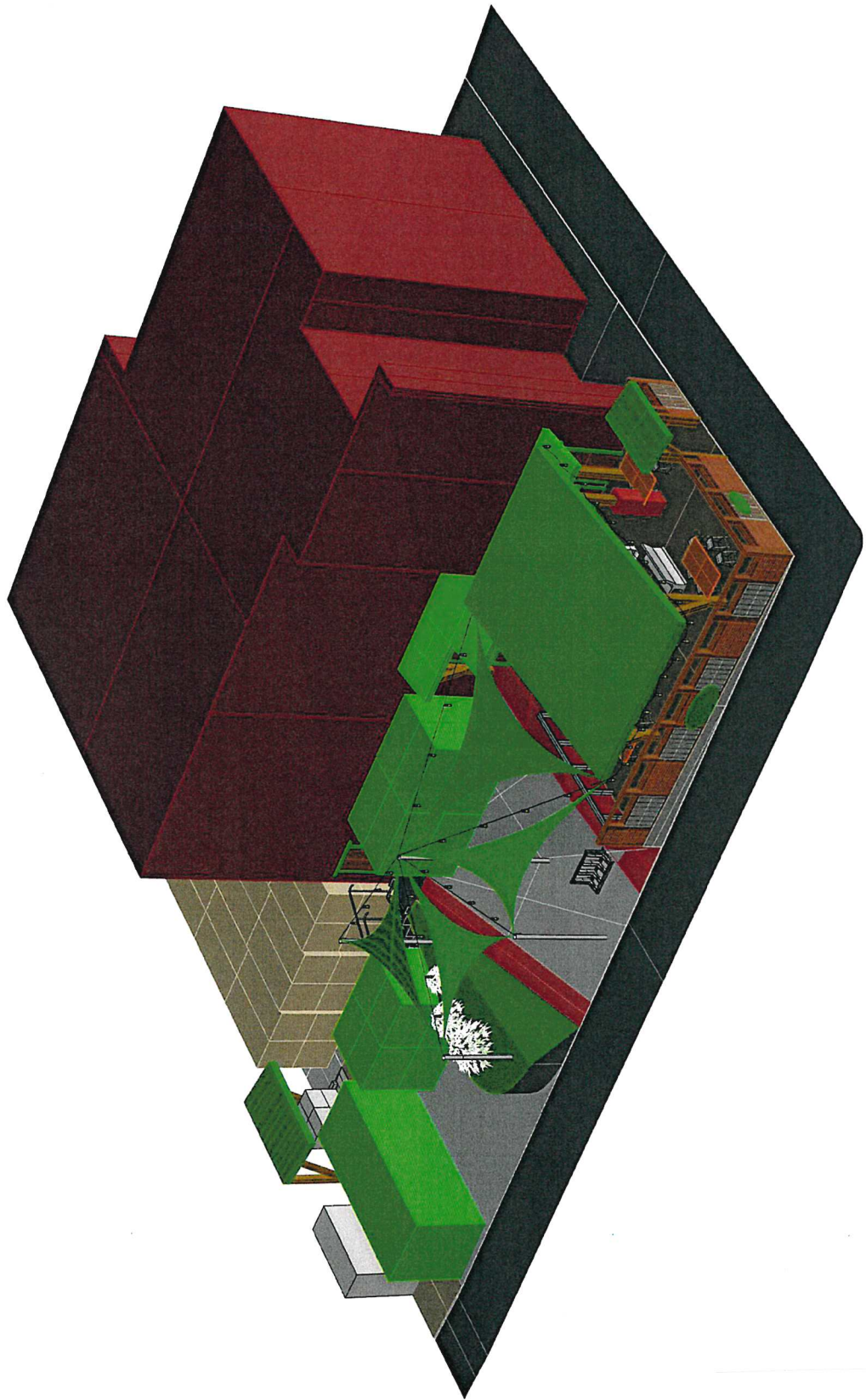


Top View - Ground Surfaces
 Scale: 1/16" = 1'

Food Court Project
 Dana Love
 dana@wetsuit.com
 541-490-1669

Food Court - Top Elevation





File No. 2020-02
Attachment "A.2"

Patio area exposed
without pergola roof.

Bathroom

Pavers

Railing

Service window

Service door

Benches

Gas fire pit

Tables

Fence



Email dated 7/1/20

Jennifer,

Below is a brief description of each of the 7 photos. In order to show the planned improvements without obstruction, we did not include the food carts in the photos.

Photo 1 - Southwest corner perspective. Pergola roof and south entrance gate roof both are green metal roofs. Pergola posts and diagonal braces are wood. Patio fence is a combination of wood and metal. Patio floor is paving stones. Fire-pit is concrete.

Photo 2 – West elevation perspective. Sail Shades are green canvas. Walkways are red brick.

Photo 3 – Service window and door perspective. Trim is green painted wood.

Photo 4 – Walkway Railing perspective. Railing is metal. Pergola posts have river rock veneer (shown in red but without "rock" detail)

Photo 5 – Trash containment area perspective. To match the pergola, the posts and diagonal braces are wood and the metal roof is green.

Photo 6 – South gate entrance perspective.

Photo 7 – Southwest corner zoomed in perspective.

Photo 8 – Northwest patio corner zoomed in perspective.

Let me know if you need any additional explanations or references.

Regards,
Dana Love – ProMotion Wetsuits

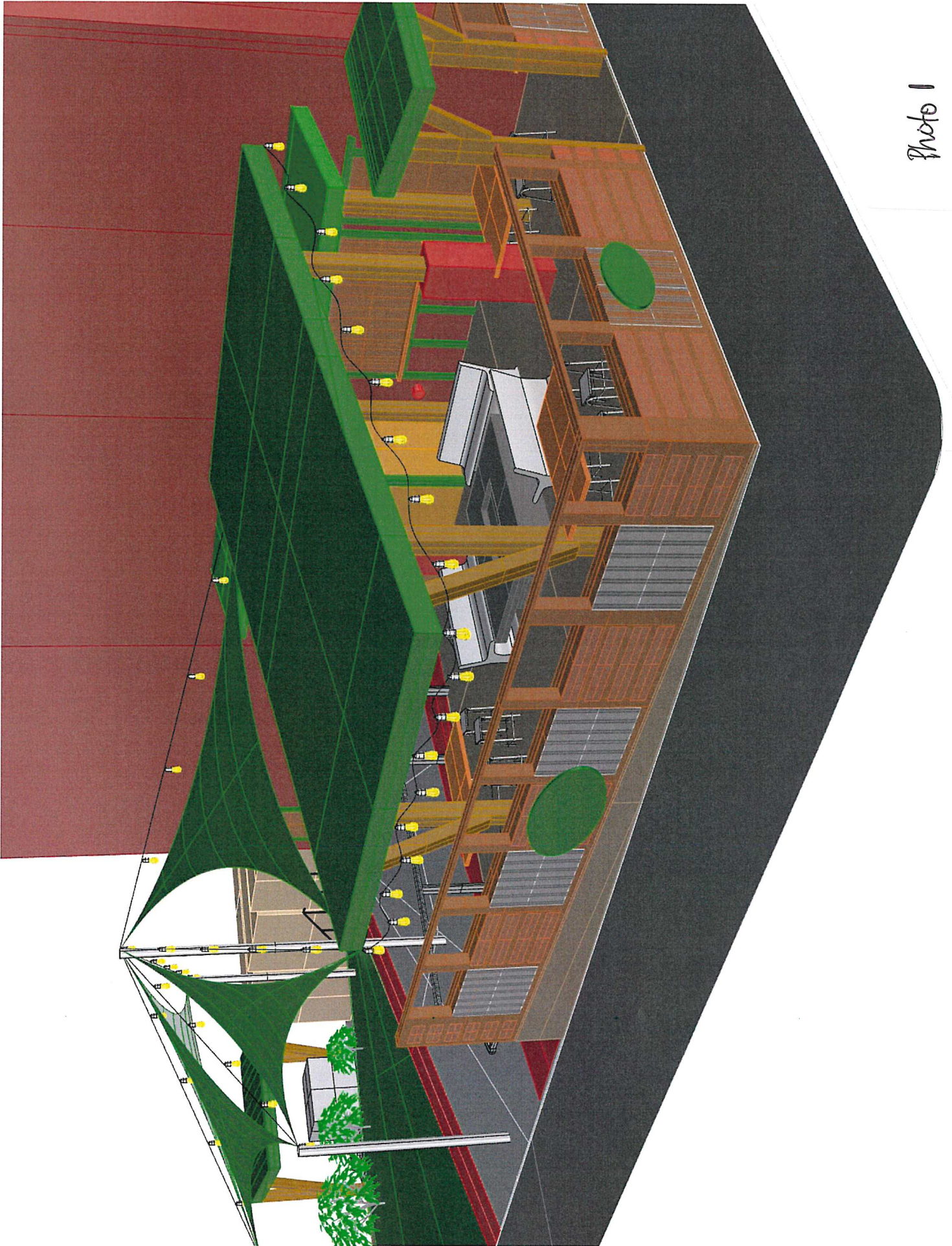


Photo 1

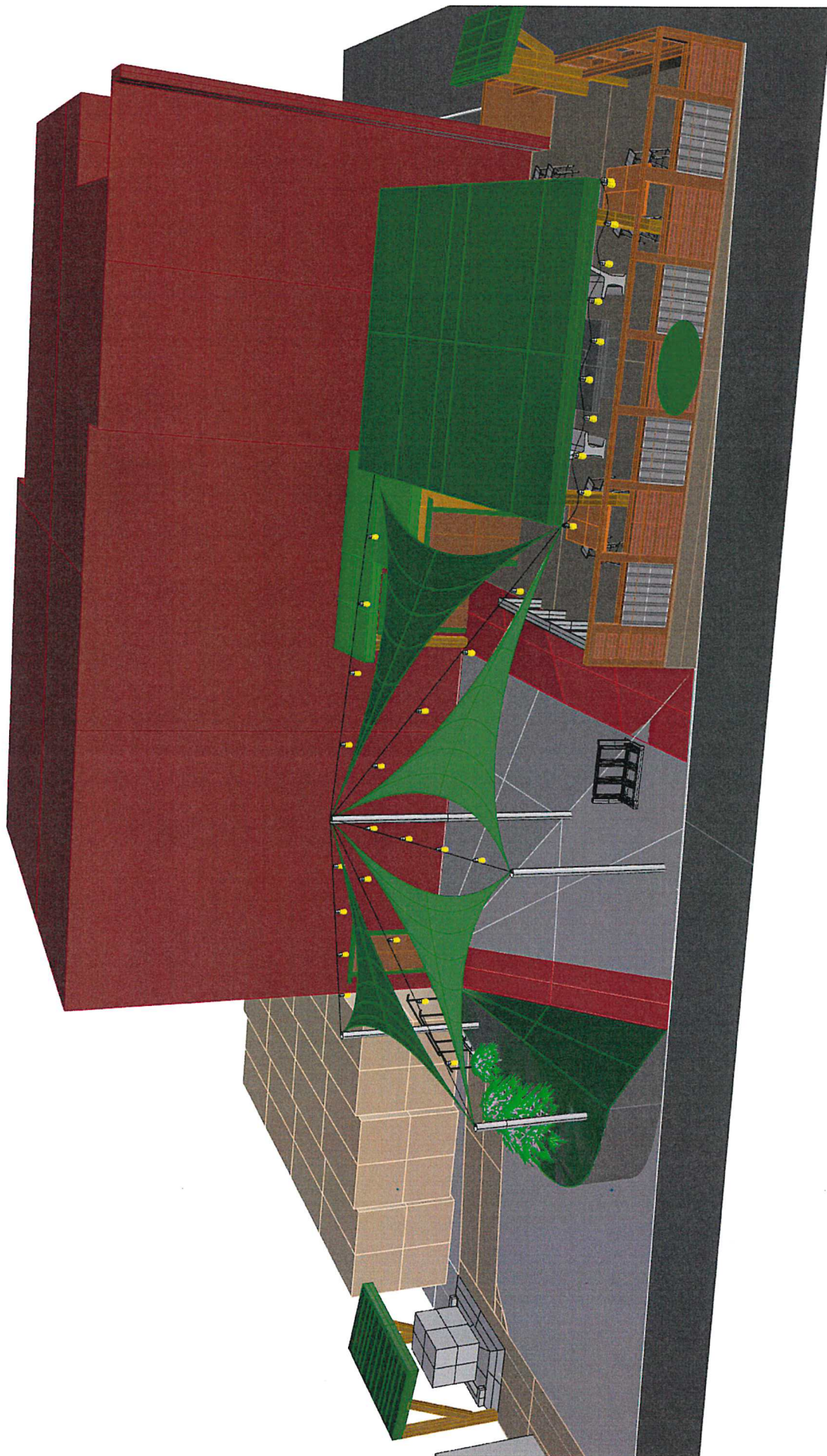


Photo 2

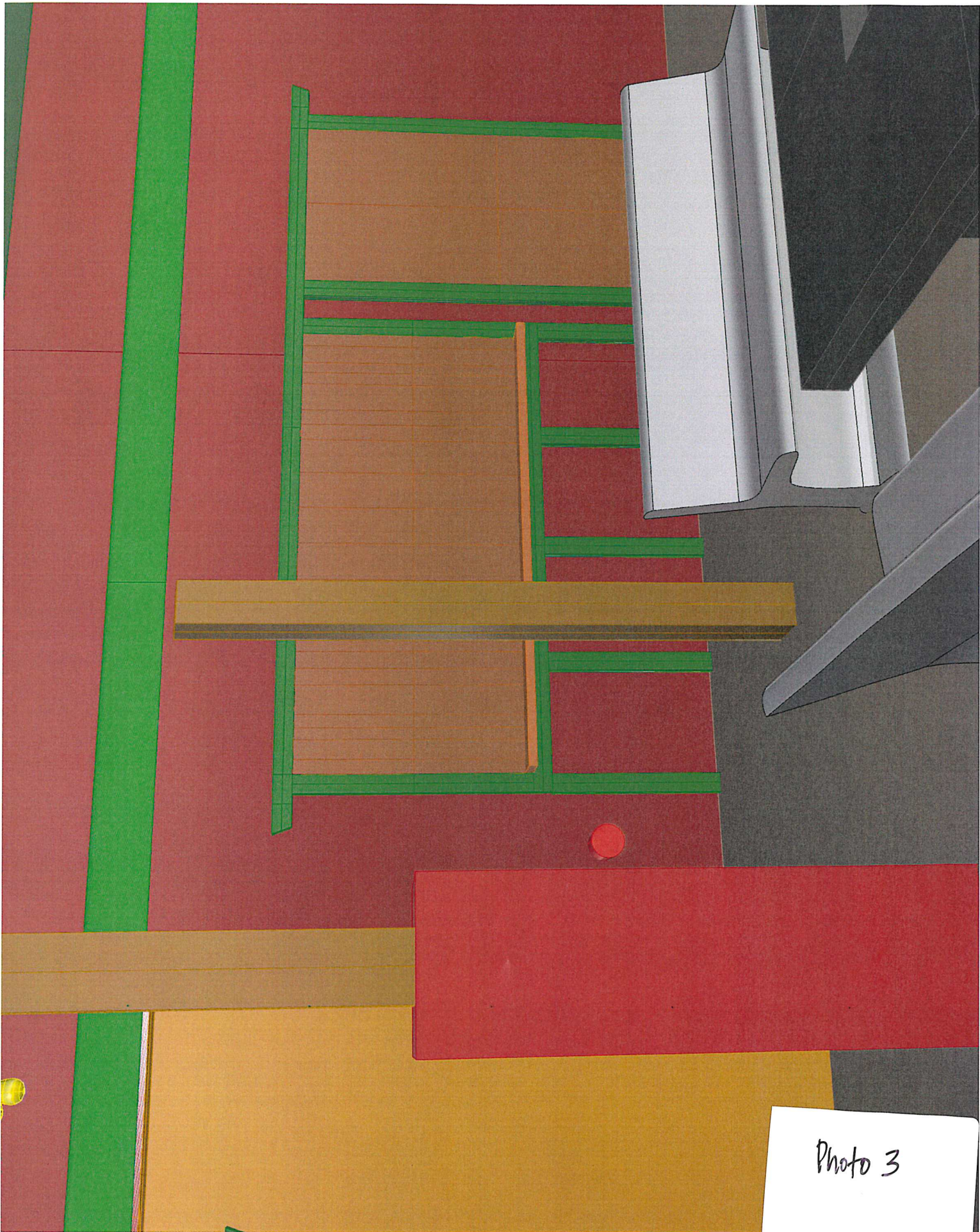


Photo 3



Photo 4

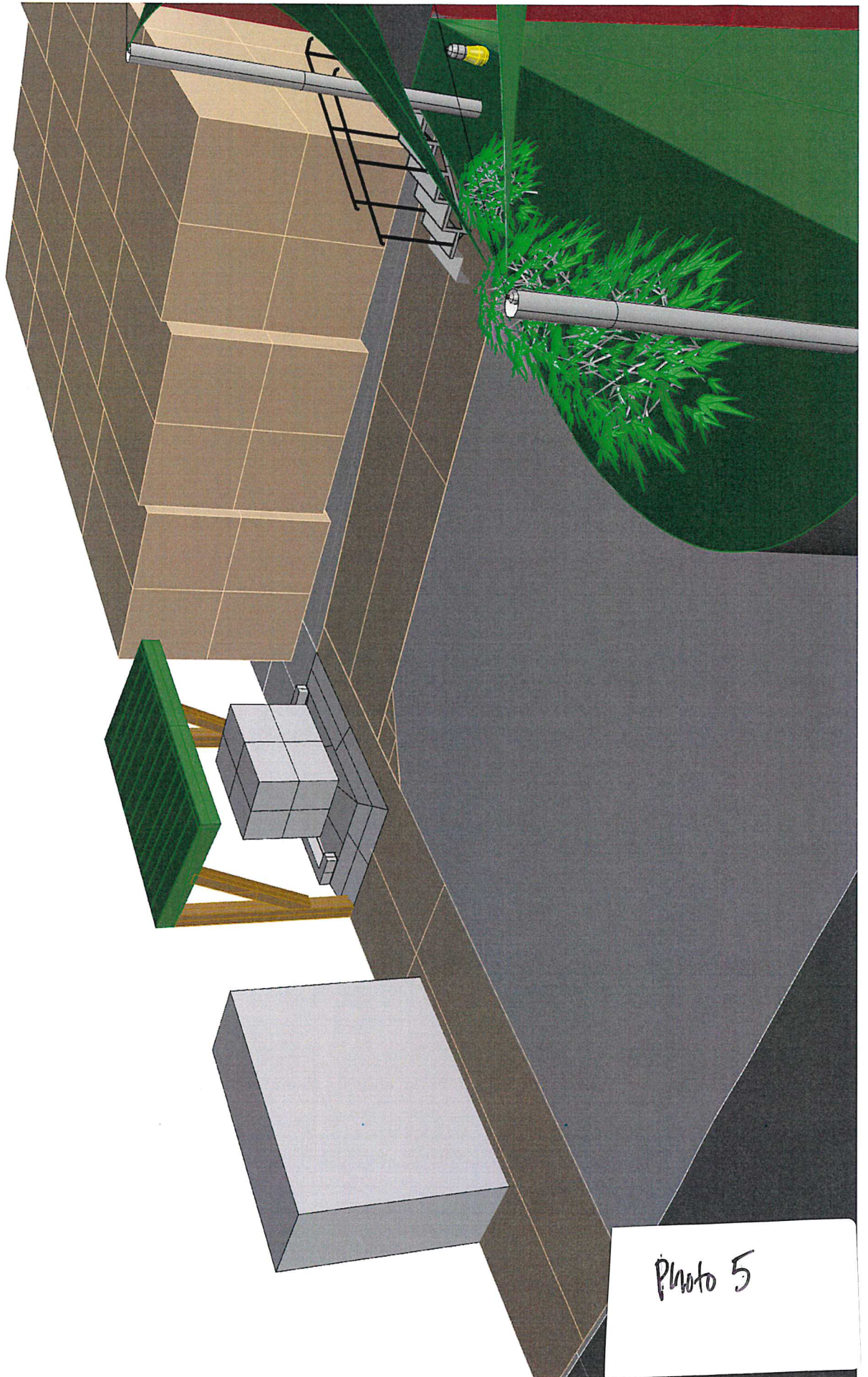


Photo 5

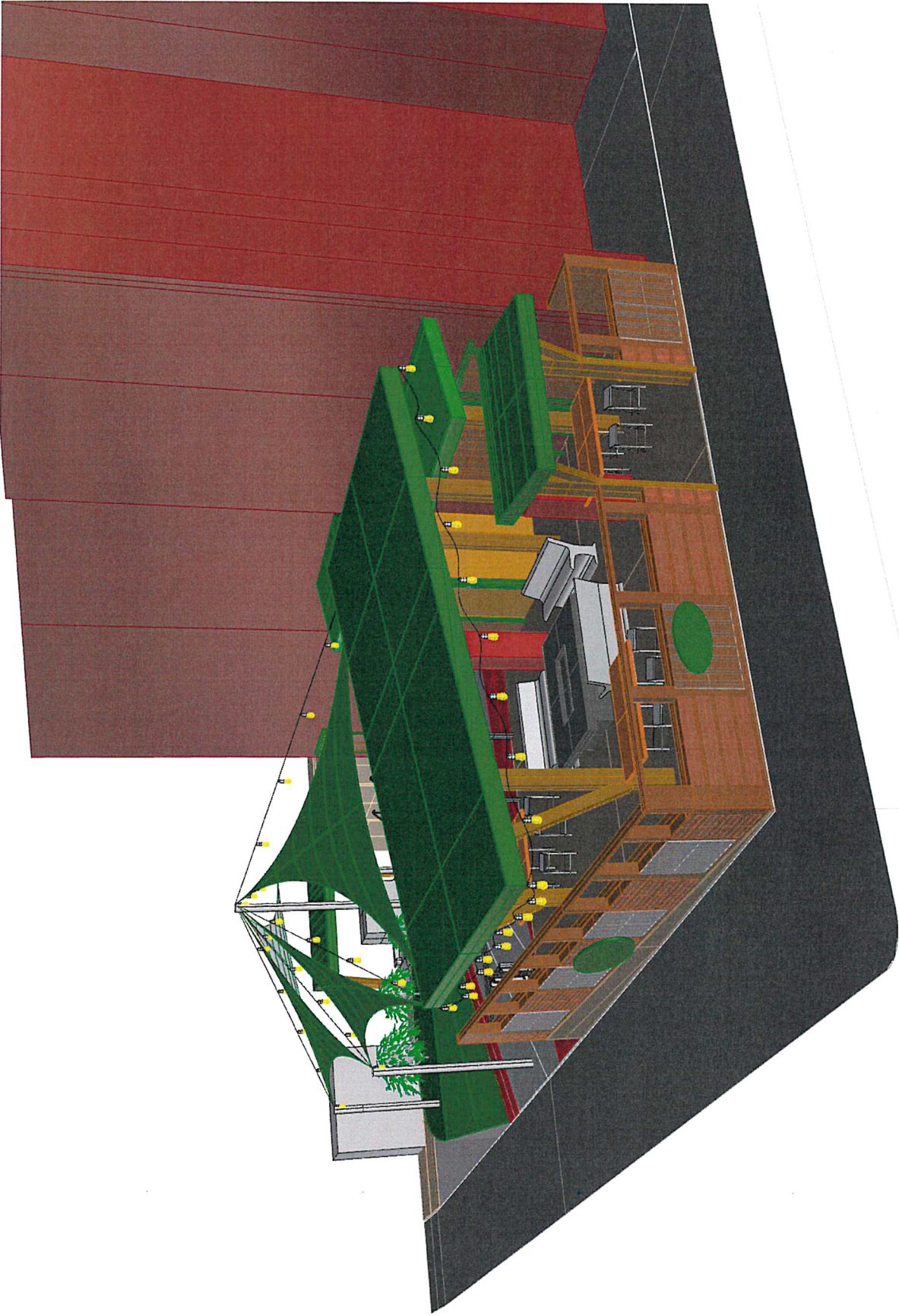


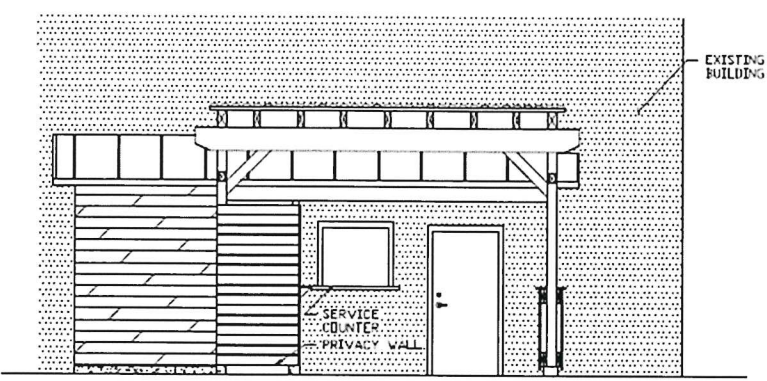
Photo 6



Photo 7

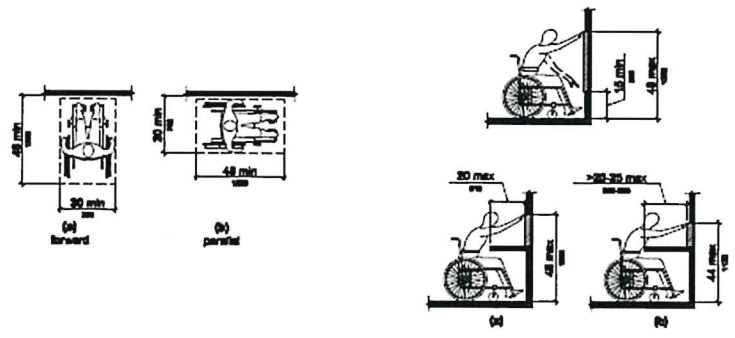


Photo 8



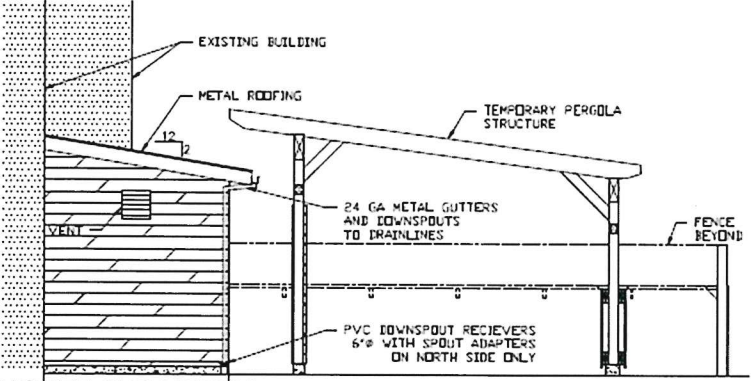
WEST ELEVATION

SCALE 1/4" = 1'-0"



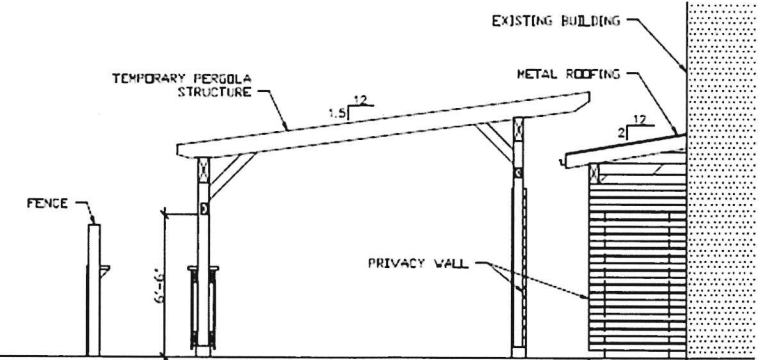
ADA REQUIREMENTS FOR SERVICE COUNTER

SCALE 1/4" = 1'-0"



NORTH ELEVATION

SCALE 1/4" = 1'-0"



SOUTH ELEVATION

SCALE 1/4" = 1'-0"

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PROMOTION
FOODCOURT RESTROOM
 416 Cascade Ave Hood River, OR

JOB NO. 178271-T02
 DATE 6/14/18
 DRAWN BY EAH CHECKED BY EAH
 #E48245 DATE



SHEET NO.
\$ 1.0

File No. 2020-02
 Attachment "A.4"

Excerpts – Landmarks Review application Narrative:

8/1/2019

In an effort to answer any questions or concerns we would like to offer the following background...

History:

We purchased the property from the Port of Hood River in 2000. There was a building on the site that was demolished in 2003. Demolished was a split level warehouse originally used for the sale of farm implements and farm supplies by the company Hood River Supply. We shared the building with other startup windsport companies before tearing it down. See photo attached -5th and Cascade Original Building

Current:

In 2004 we were approved to build a new building on the site. The new building was done in an architectural design to resemble turn of the century brick building on the sides facing the street. The new building was designed to be built in phases. The first phase built covers roughly 40% of the property. The remaining phases once completed will cover the balance of the property. It is our intention to finish the remaining phases as was originally approved. See photo attached – ProMotion Building as completed – West Elevation.

Proposal:

Until the later phases of construction become economically feasible, we are looking to make use of the vacant lot space, and are proposing an improved area for food carts. Currently the lot has food carts on site with no amenities. We plan to add the following...a bathroom, a pergola, sail shade canopy's, a fenced and paved patio, picnic benches, string lighting for evenings, and a gas fire pit. See documents submitted with application.

We would appreciate input and/or suggestions as to how we can adapt our food court plan with regards to the Landmarks Review.

10/21/2019

I have a vision of what I want to develop based on trends seen in other Oregon cities. I have spent many hours and have given much consideration to this project. I have tried to include elements of design already existing in the downtown area.

Please find attached an updated perspective view – Food Court Perspective View Oct 17-2019.

11/18/2019

Re: Landmarks Review -

At your recommendation I emailed Joy Sears providing her with background, site history, photos, and drawings of our proposed Food Cart Courtyard. I have not yet heard back from her but will try

again.

1/23/2020

History:

The property had on it a split level warehouse and was owned by the Port of Hood River. We purchased the property from the Port in 2000. We shared the space with startup windsport companies before demolishing the building in 2003 to make room for our new building. See photo attached of the building that was demolished - 5th and Cascade Original Building

Current:

In 2004 we were approved to build a new building on the site. The new building was done in an architectural design to resemble turn of the century brick buildings common to early downtown Hood River. The new building was designed to be built in phases. The first phase built covers roughly 40% of the property. The remaining phases once completed will cover the balance of the property. It is our intention to finish the remaining phases as were originally approved. See photo attached – ProMotion Building as completed – West Elevation.

Proposal:

Until the later phases of construction become economically feasible, we are looking to make use of the vacant corner lot space, and are proposing an improved area for food carts. Currently the lot has food carts on site with no amenities. We plan to add the following...an outside bathroom, a pergola, shade canopy, a fenced and paved patio, benches, tables, string lighting, and a gas fire pit. See documents submitted with application.

Narrative:

If we were adding the next phase to our existing building, it would be a simple matter of following the construction details as were originally approved. Instead we are proposing minimal improvements as a temporary use of the empty corner lot space.

We contacted Joy Sears with the Oregon State Historic Preservation Office (SHPO) for some guidance. We sent her drawings and details of our project. The following is an excerpt from her reply..."Our office isn't familiar with the historic ordinance or design guidelines but generally the design guidelines are building specific. Your proposed project while it has a building addition competent with the restroom, the rest of the project is more street furniture, hardscaping, and landscaping."

This reply is consistent with our original argument that a Landmarks Review for this project is unreasonable due to the fact that it "landscaping" with a restroom and not a full blown building addition.

Still, to try and satisfy the Landmarks Review requirement, we have looked at historical buildings in downtown Hood River and reviewed photo archives from the Hood River Museum for inspiration. From this, the following design features have been added or

upgraded...

- 1>Diagonal Braces have been added to the pergola posts.
- 2>River Rock Columns have been added to pergola posts.
- 3>Exterior Trims have been added to the bathroom addition, proposed exterior doors, and service window area.
- 4>The color scheme uses colors and tones to match the existing building and landscaping. These include red's, brown's, and green's.
- 5>Brick Walkways match the existing building.
- 6>Storage Containers at the rear of the building will be screened from view by food cart placement on the lower end of the lot.
- 7>The Trash Containment Area design matches the pergola structure.

As of January 20, 2020 all design elements mentioned above have been added to our drawings. Let me know if you need updated drawings showing these changes. We can also provide 3D viewing of the proposed project from any camera angle.

Photos mentioned above should already be in your files:
ProMotion Building as completed – West Elevation.JPG
5th and Cascade Original Building.JPG

File: Cascade and 5th Site Plan Review 6
JANUARY 23, 2020

Jennifer,

In reply to your "Incomplete Letter" of November 14, 2019 we have updated our Landmarks Review application (below).

We are still waiting for a Traffic Assessment Letter from Charbonneau Engineering.

Regards,
Dana Love
ProMotion Wetsuits

Landmarks Review application -

The Landmarks Review application must include a detailed written analysis demonstrating how the proposed development meets the criteria for new construction in the Historic District, HRMC 17.14.110.G. Specifically, the development must be "compatible with the design of the historic landmarks in the district" taking into account scale, style, height, and architectural detail and materials AND the new construction must be "consistent with the typical location and orientation of similar structures ...in the district considering setbacks, distances between structures, location of entrances, and similar [s]iting considerations." A copy of the standards in HRMC 17.14.110, Review of New Construction in a Historic District, is attached for your reference.

The written analysis should include the storage containers in the scope of work. The city has no record of permitting the existing storage containers. Evaluation of the containers will be included in the Landmarks Review application.

History:

The property had on it a split level warehouse and was owned by the Port of Hood River. We purchased the property from the Port in 2000. We shared the space with startup windsport companies before demolishing the building in 2003 to make room for our new building. See photo attached of the building that was demolished - 5th and Cascade Original Building

Current:

In 2004 we were approved to build a new building on the site. The new building was done in an architectural design to resemble turn of the century brick buildings common to early downtown Hood River. The new building was designed to be built in phases. The first phase built covers roughly 40% of the property. The remaining phases once completed will cover the balance of the property. It is our intention to finish the remaining phases as were originally approved. See photo attached – ProMotion Building as completed – West Elevation.

Proposal:

Until the later phases of construction become economically feasible, we are looking to make use of the vacant corner lot space, and are proposing an improved area for food carts. Currently the lot has food carts on site with no amenities. We plan to add the following...an outside bathroom, a pergola, shade canopy, a fenced and paved patio, benches, tables, string lighting, and a gas fire pit. See documents submitted with application.

Narrative:

If we were adding the next phase to our existing building, it would be a simple matter of following the construction details as were originally approved. Instead we are proposing minimal improvements as a temporary use of the empty corner lot space.

We contacted Joy Sears with the Oregon State Historic Preservation Office (SHPO) for some guidance. We sent her drawings and details of our project. The following is an excerpt from her reply..."Our office isn't familiar with the historic ordinance or design guidelines but generally the design guidelines are building specific. Your proposed project while it has a building addition competent with the restroom, the rest of the project is more street furniture, hardscaping, and landscaping."

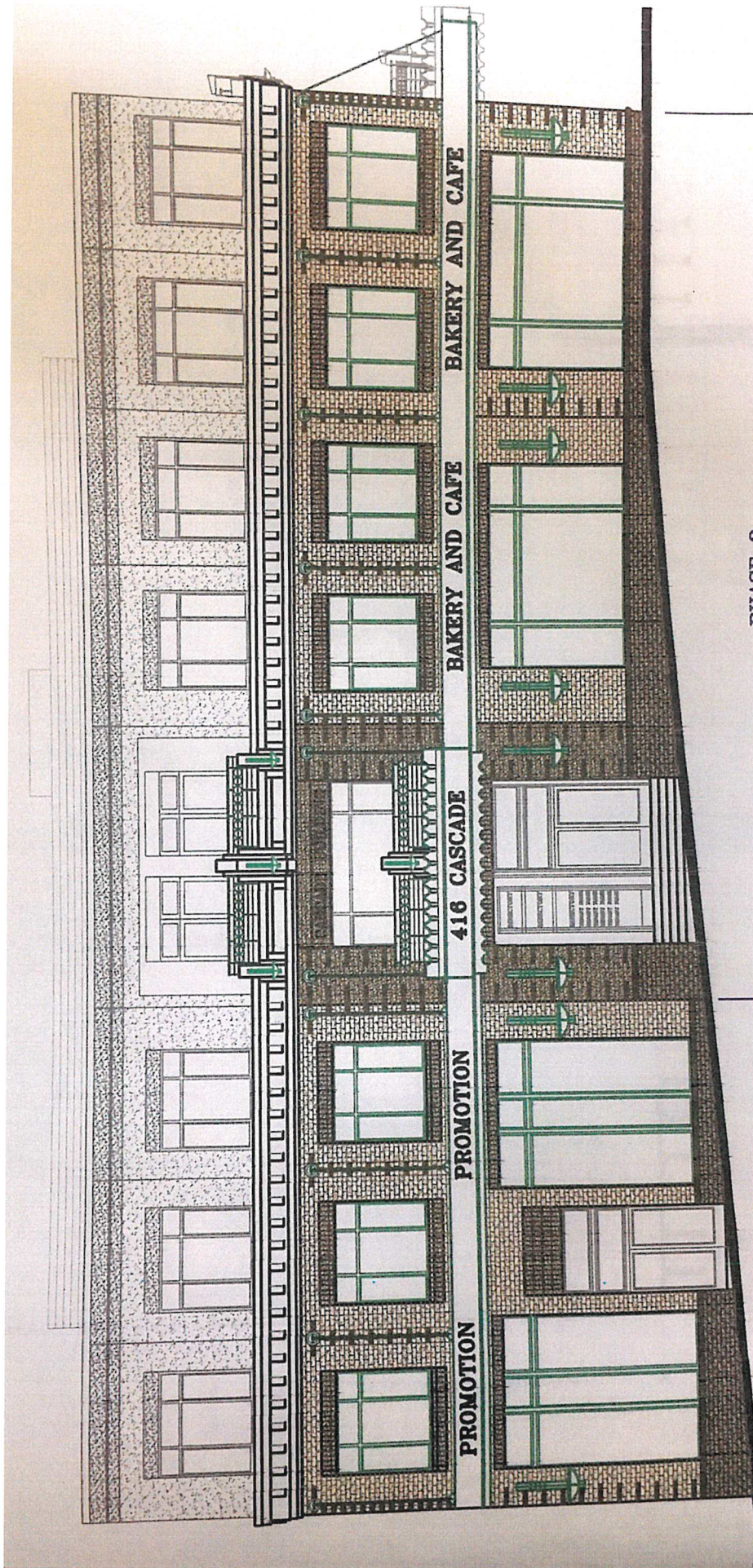
This reply is consistent with our original argument that a Landmarks Review for this project is unreasonable due to the fact that it "landscaping" with a restroom and not a full blown building addition.

Still, to try and satisfy the Landmarks Review requirement, we have looked at historical buildings in downtown Hood River and reviewed photo archives from the Hood River Museum for inspiration. From this, the following design features have been added or upgraded...

- 1>Diagonal Braces have been added to the pergola posts.
- 2>River Rock Columns have been added to pergola posts.
- 3>Exterior Trims have been added to the bathroom addition, proposed exterior doors, and service window area.
- 4>The color scheme uses colors and tones to match the existing building and landscaping. These include red's, brown's, and green's.
- 5>Brick Walkways match the existing building.
- 6>Storage Containers at the rear of the building will be screened from view by food cart placement on the lower end of the lot.
- 7>The Trash Containment Area design matches the pergola structure.

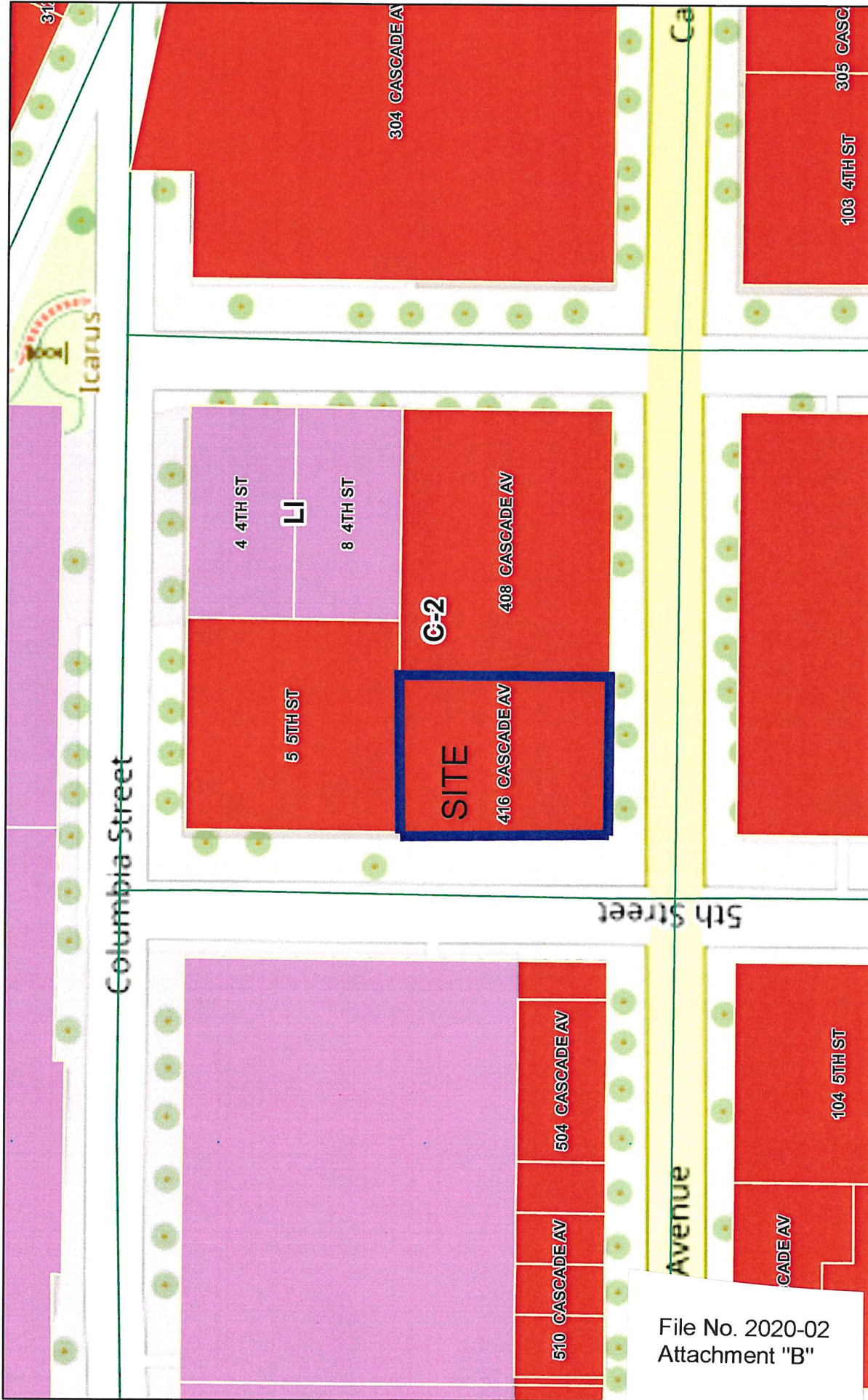
As of January 20, 2020 all design elements mentioned above have been added to our drawings. Let me know if you need updated drawings showing these changes. We can also provide 3D viewing of the proposed project from any camera angle.

Photos mentioned above should already be in your files:
ProMotion Building as completed – West Elevation.JPG
5th and Cascade Original Building.JPG



PHASE 2

Location Map File No. 2020-02

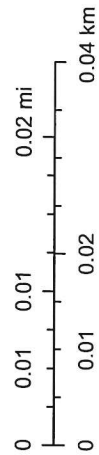


File No. 2020-02
Attachment "B"

6/4/2020, 3:50:11 PM

- City Limits
- Urban Growth Boundary
- Hood River SITUS Addresses
- QuarterQuarterGRID
- C-2
- LI

1:1,128



© OpenStreetMap (and) contributors, CC-BY-SA

Jennifer Kaden

From: SEARS Joy * OPRD <Joy.Sears@oregon.gov>
Sent: Tuesday, November 26, 2019 1:00 PM
To: Jennifer Kaden
Subject: FW: 5th and Cascade Hood River – Food Cart Courtyard

FYI – I shared a short list of 3 preservation consultants with him. I hope that helps.

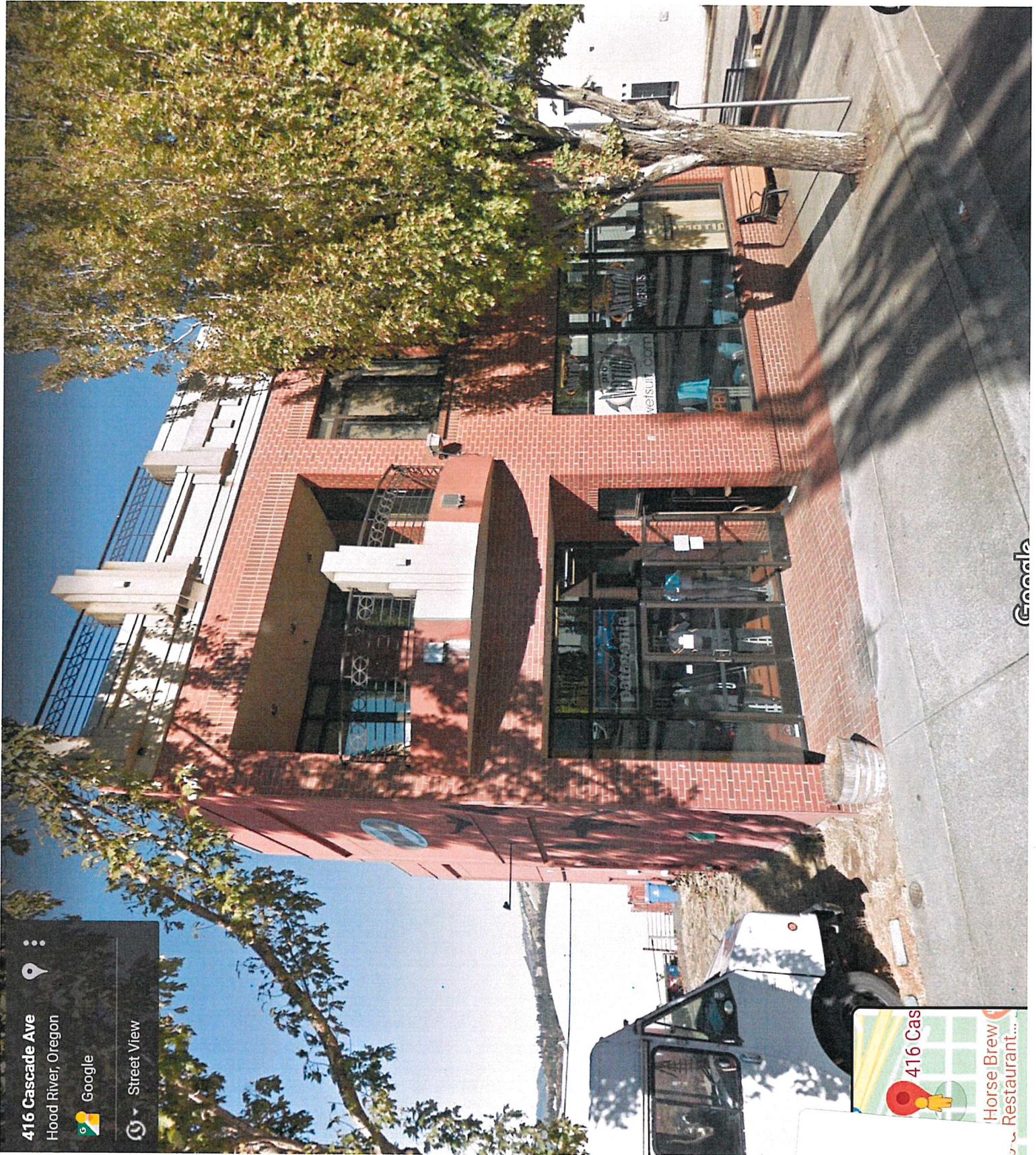
From: dana@wetsuit.com [mailto:dana@wetsuit.com]
Sent: Tuesday, November 26, 2019 10:20 AM
To: SEARS Joy * OPRD
Subject: Re: 5th and Cascade Hood River – Food Cart Courtyard

Joy,

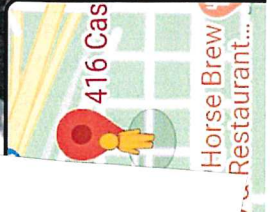
I appreciate your offer to share a short list of preservation consultants. Looking forward to your reply.

Dana Love

File No. 2020-02
Attachment "C"



416 Cascade Ave
Hood River, Oregon
Google
Street View

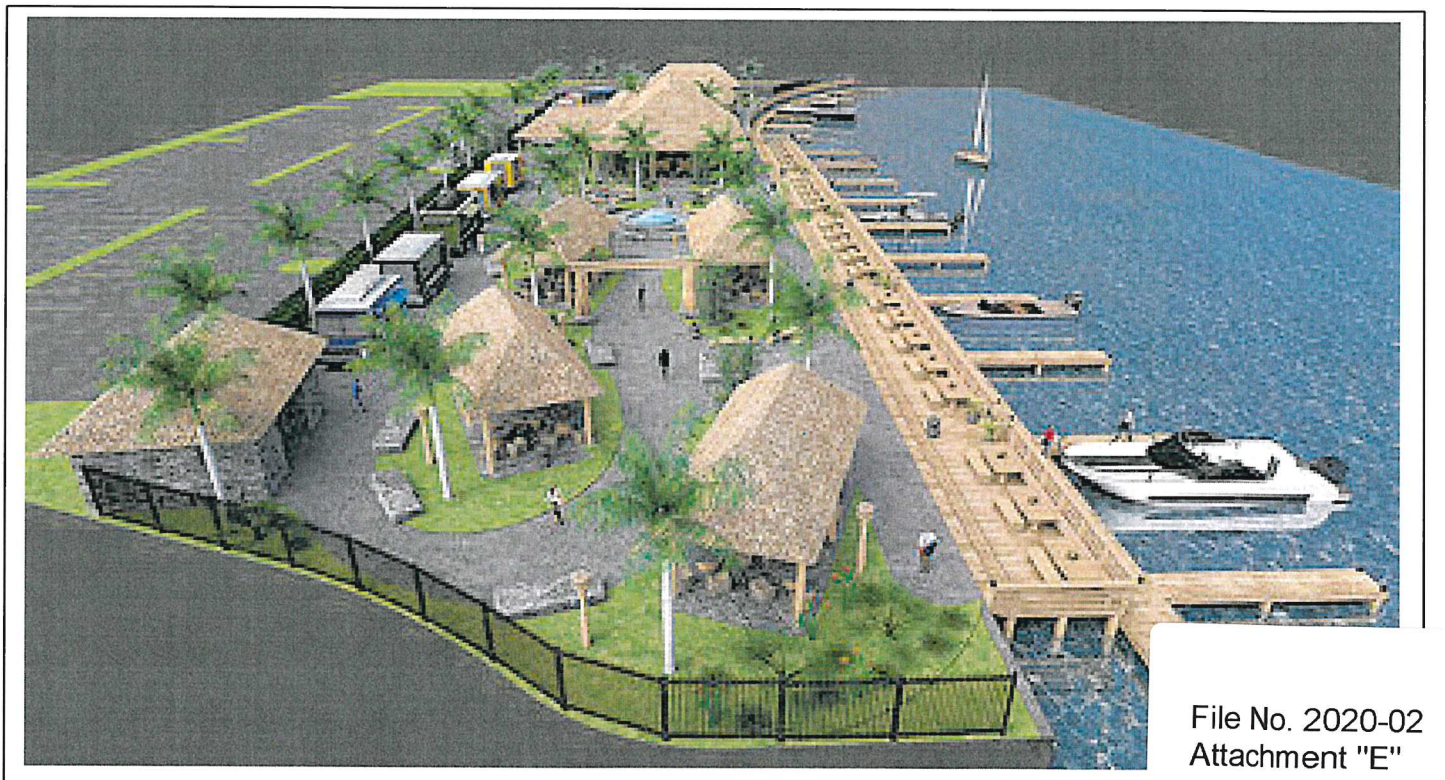


File No. 2020-02
Attachment "D"

This image shows a food cart park design with food trucks set back from the public street and use of fencing and landscaping to screen the use:

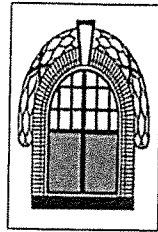


This image shows a food cart park with a cohesive design and a layout that de-emphasizes the food trucks:



File No. 2020-02
Attachment "E"

DESIGN GUIDELINES



Hood River *Local Downtown Historic District*



DESIGN GUIDELINES

Hood River Local Downtown Historic District

INTRODUCTION

The purpose of these advisory Design Guidelines are to protect the historic character of Hood River's local Commercial Historic District by providing rehabilitation and new construction guidelines to owners of buildings in the District. Property owners can use these guidelines in making compatible design decisions about their historic buildings, and in developing applications to the Hood River Landmarks Review Board (LRB). These design guidelines will also provide assistance to the City and LRB as they review alteration and new construction requests within the District. The Design Guidelines are designed to encourage Hood River residents and owners of historic properties to appreciate and preserve the local architecture and history which helps define the unique character of the community.

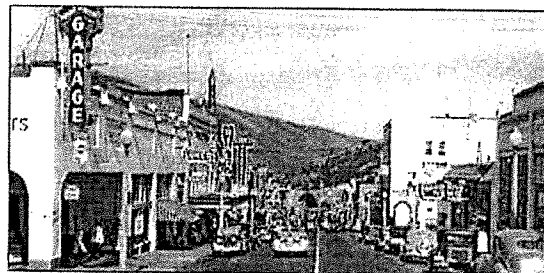
HISTORIC VIEW OF HOOD RIVER

The Hood River Local Downtown Historic District encompasses significant buildings in the City's history which date from 1886 to 1937. The District has a concentrated collection of buildings reflecting the early development of Hood River as a leading trading and transportation center for the Hood River Valley. Downtown Hood River not only served the local community, but also handled the regional trade of the farmers, orchardists, and loggers who came to town to ship their commodities, shop for goods, recreate, and conduct business.

Mary and Nathaniel Coe were "Dog River's" first permanent Euro-American settlers, claiming 319.92 acres in 1854. The Coe home served as the community center, courthouse, church, and funeral parlor. Mary Coe is responsible for renaming the settlement of Dog River to Hood River after majestic Mount Hood to the south. Dependent on the Columbia River for transportation, Hood River developed slowly until 1882 when the railroad was completed through the Columbia Gorge. The railroad connected the town with other towns across the nation. Hood River was platted in 1881 as a result of the coming of the railroad. With its completion, the commercial core of the community refocused from the river to the railroad depot and beginning a new period of expansion. One and two-story wooden buildings lined Oak Street, the main street in town. The town further expanded in 1895 when the town was officially incorporated. The population more than tripled from 201 people in 1890 to 622 people in 1900.

The growth continued after the turn of the century as people from all over acquired the beautiful and fertile land of the Hood River Valley. The completion of Mt. Hood Railroad from Hood River to Parkdale in 1910 improved accessibility to the productive timber and orchard lands in the valley. The buildings in downtown Hood River reflect the influx of wealth that occurred in the first two decades of the 20th century as local industries grew. Between 1901 and 1914 half of the commercial buildings were built. These brick structures represent the second construction phase in the town's history, when brick buildings gradually replaced smaller wooden structures. Fruit warehouses, fraternal buildings, depots, and commercial buildings were erected during this period of rapid economic growth.

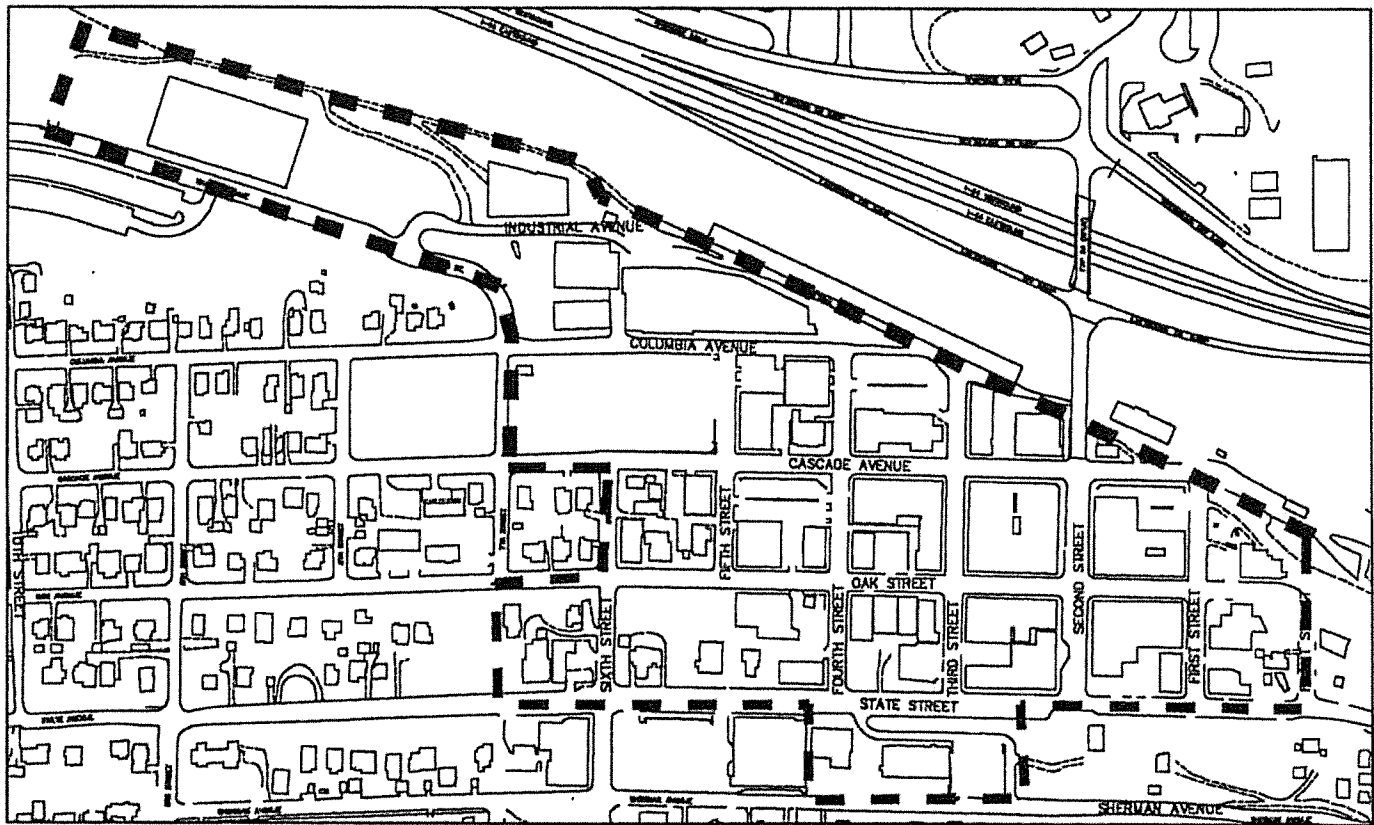
The next growth period occurred as a result of the introduction of the automobile and the completion of the Columbia River Highway from Portland to Hood River in 1916. The automobile changed the face of Hood River as service stations and car dealerships were built in the mid-teens through the 1930s. These buildings were generally made of concrete or hollow clay tile and one-story in height. City Hall, the U.S. Post Office, and fruit, canning, and cold storage warehouses were erected during this early age of the automobile. The historic buildings in downtown depict Hood River's rich past based in the timber, fruit, and tourism industries. These industries are still a part of Hood River's economic base.



DESIGN GUIDELINES
Hood River Local Downtown Historic District

LOCAL DOWNTOWN HISTORIC DISTRICT

The buildings in downtown are significant for their association with the development of Hood River as a leading commercial and trading center for Hood River County. As a way of recognizing and protecting the historic character of downtown Hood River, a local historic district was established in 1994 under the provisions of the Historic Preservation Ordinance Chapter 17.14 of the Hood River Municipal Code (HRMC). Under the ordinance new construction on vacant parcels and major alterations to contributing properties within the District are reviewed by the Landmarks Review Board to maintain the historic integrity of the downtown (see Appendix B for a complete list of the contributing buildings in the District).

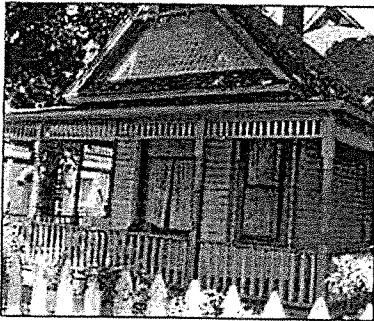


Key:  District boundary

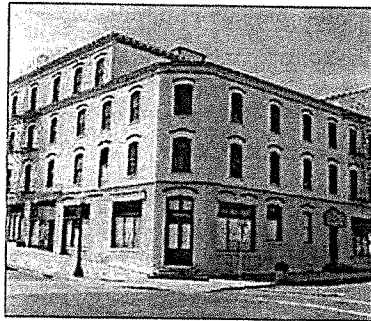
ARCHITECTURAL STYLES

Similar types of commercial buildings provide continuity to the downtown streetscape; the different styles of these commercial structures create visual variety and help distinguish one building from another. These differences reflect what was popular at the time of construction, the use of the building, and/or the tastes of the owner, builder, or architect.

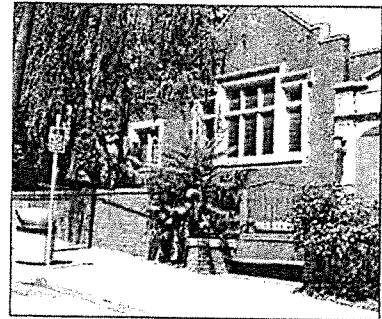
The historic buildings in the downtown Hood River date from 1886 to 1937, and show a variety of different building styles. Although primarily vernacular in character, the buildings display elements of various styles. Learning about the style of the building can help answer preservation questions including those about the original treatments, color schemes, and what should replace missing elements.



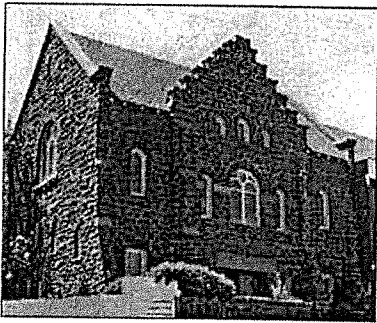
QUEEN ANNE COTTAGE
(Parker Residence, c. 1900: 525 State)



ITALIANATE
(Hotel Waucoma, 1904: 106 Second)



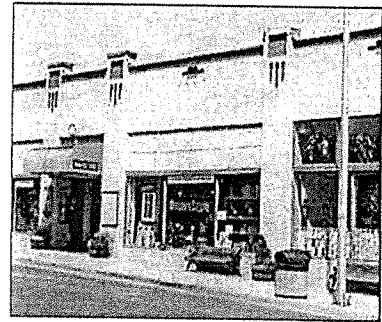
JACOBETHAN
(Hood River County Library, 1913: 502 State)



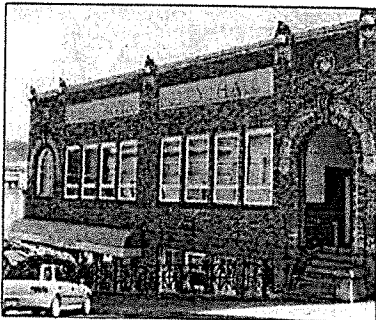
RICHARDSONIAN ROMANESQUE
(Congregational Church, 1913: Fourth & State)



20TH CENTURY COMMERCIAL
(Franz Hardware, 1908: 116 Oak)



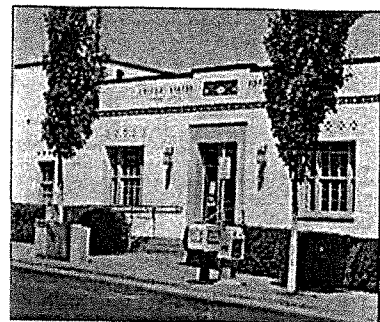
COMMERCIAL-AUTO RELATED
(Hood River Garage, 1919: 202 Cascade)



AMERICAN RENAISSANCE
(City Hall, 1920: 207 Second)



EGYPTIAN
(Butler Bank, 1924: 301 Oak)



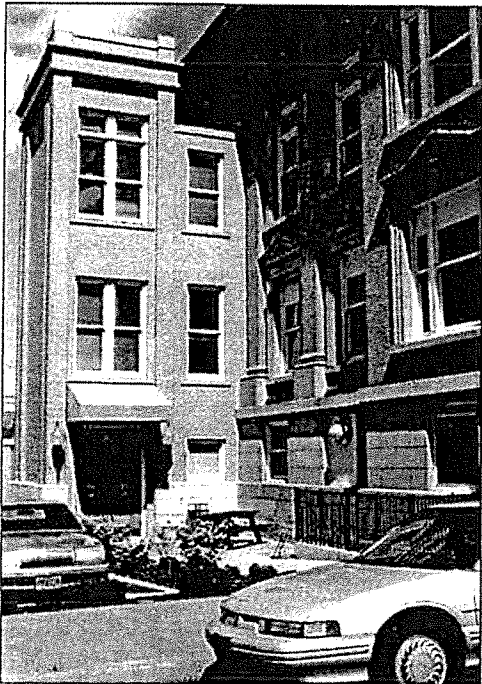
MODERNE
(U.S. Post Office, 1935: 408 Cascade)

New Additions

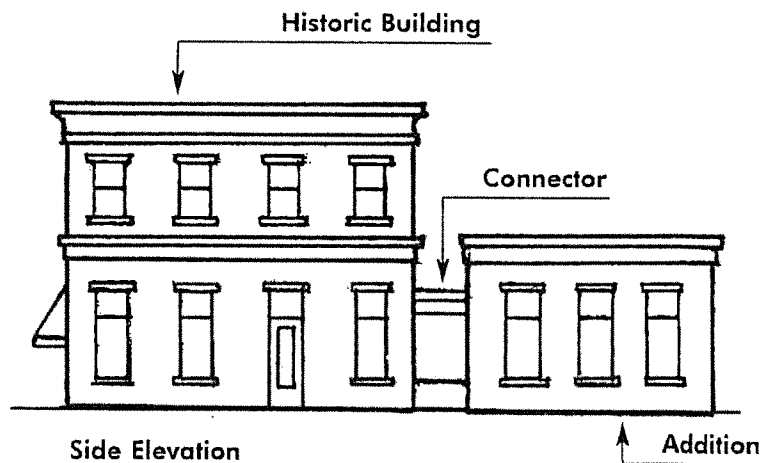
A modern addition to a historic building is the most sensitive and difficult design issue to manage. Few of the historic buildings in downtown Hood River have recent additions. However, changing needs may increase the desire for expansion of these historic structures.

Guidelines:

- A. Preserve significant historic materials and features.
- B. Avoid attaching additions on primary (or street) elevations.
- C. Design the addition to be subordinate to the historic building.
- D. Minimize the loss of historic material by linking the new addition to the historic building by a connector; only the connecting passageway would penetrate the historic wall.
- E. Consider setting the connector back from the historic building's wall plane so the form of the historic building can be distinguished from the new addition.
- F. Protect the historical significance of the building by making a visual distinction between the old and new.
- G. Design new additions that are compatible with the size, scale, proportions, color, material, and character of the historic building, and surrounding buildings in the block.
- H. Design additions so that they are smaller in scale so as not to overpower the historic building.
- I. Set back an additional story from the roof edge to ensure the historic building's profile is not radically changed.
- J. Avoid creating a false historical appearance that does not relate to the surrounding buildings or history of Hood River.



Wasco County Courthouse: Compatible elevator addition on rear elevation.



Rear addition to historic building.

New Construction Guidelines

The following elements are also applicable to rehabilitation projects.

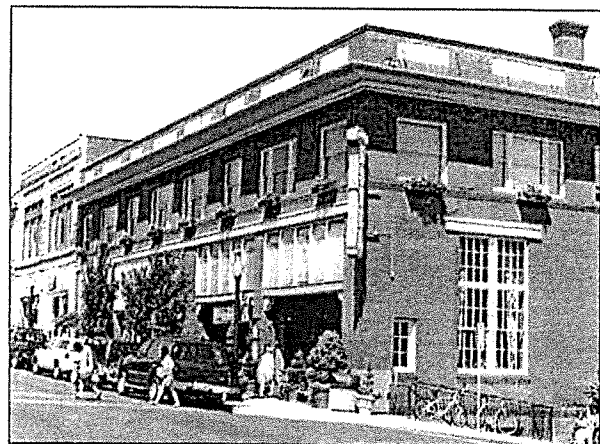
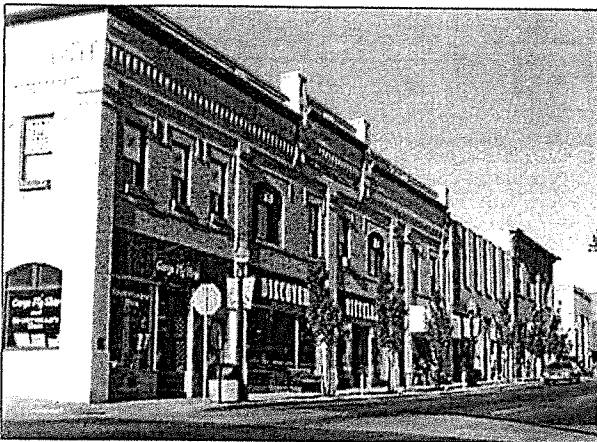
Design guidelines for a historic district should not dictate certain styles for new buildings because most areas exhibit an evolution of architectural styles. These design guidelines emphasize compatibility, context, and design elements rather than styles, which allow for a broad and flexible approach to new construction within an historic district. The design guidelines for new construction emphasize building characteristics that may be shared with old and new. Attention to these elements encourages the design of buildings that clearly are new, yet do not disrupt the historic streetscape of the District. The following are some of the elements to consider when designing new buildings within the downtown Historic District.

Streetscape and Setbacks

Hood River downtown has unique characteristics that define the City's streetscape. The 60 to 80 ft. streets including 10-15 ft. sidewalks, low to medium building heights, and large storefronts reflect the historic character of the town. The majority of the historic buildings in downtown Hood River are flush with the sidewalk, abut one another (except where parking lots have replaced buildings), and have recessed entries. Some of the buildings in the District vary from the traditional setback. Examples of these include the Butler Bank (301 Oak), the U.S. Post Office, the Hood River County Library, and the buildings in the District that once were residential in use. Any new construction should address the street in a manner consistent with neighboring structures and overall street form and character.

Guidelines for Traditional Commercial Setbacks:

- A. Front new construction to the street.
- B. Abut new construction with the adjacent buildings if traditional pattern has been established.
- C. Recess entries slightly from the building's edge creating a protected area.



Traditional setbacks along Oak Street; the buildings are flush with sidewalk and abut one another.

Building Height

Generally the historic resources in the District are one and two-story buildings with the exception of a few three and four-story structures (Stewart Hardware, 202 State; the Paris Fair/IOOF Hall, 315 Oak; and the Hotel Waucoma, 106 Second). The three and four-story buildings are anchor buildings, located on corner lots. The maximum downtown building height allowed in the HRMC is 45 ft. and the average height of a downtown building is between 25 ft. and 33 ft. A majority of the commercial buildings have parapets which make the buildings appear higher (see "Roof Form" guidelines page 11).

Guidelines:

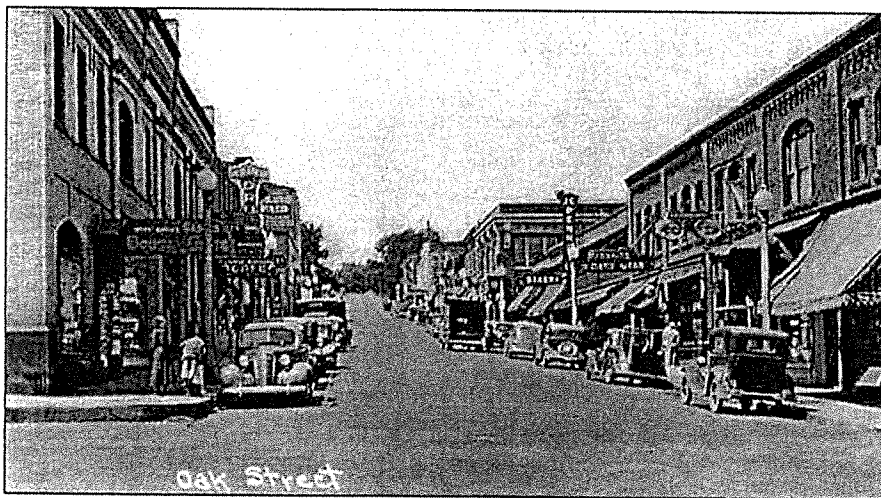
- A. Maintain the range in building heights from one to two-stories; this variation creates a visually interesting street as well as retaining the character of the historic streetscape.
- B. Design the height of the buildings within the range of heights found on the immediate block.
- C. Maintain the height to width ratio with other buildings in the immediate area.
- D. Maintain floor to floor height with other buildings in the immediate area (first floors are generally higher).
- E. Encourage the use of simply designed parapets, in the building design (see "Roof Form" guidelines).

Building Width

Building width is one of the most predominant visual qualities in historic downtown. Downtown buildings were often platted into relatively narrow and deep lots. These widths often characterize how a downtown appears. Building widths in downtown Hood River reflect this patterning and vary from 27 ft. to 100 ft. wide. The smaller historic one-story buildings range in width from 27 ft. to 50 ft. wide and the larger two, three-and four-story anchor or corner buildings are about 50 ft. to 100 ft. wide.

Guidelines:

- A. Construct new buildings from side lot line to side lot line.
- B. Maintain the height-to-width ratio with other buildings in the immediate area.



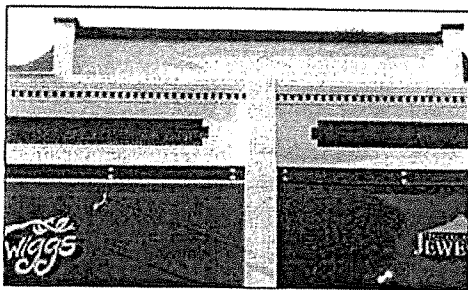
Looking west from Second and Oak Streets, c. 1930.

Roof Forms

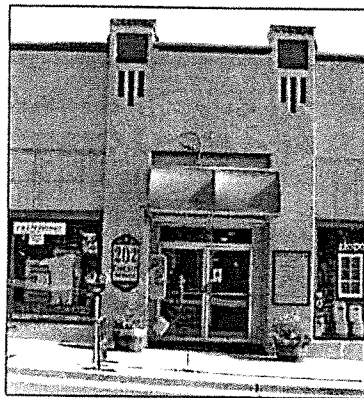
The roofs of the commercial buildings in the District are generally flat or sloped and hidden by parapets or false fronts. This is in contrast to visible pitched roofs in residential areas. Parapets add character to a building and are often stepped and embellished with cornices, special facing material, or decorative details. Parapets are a prominent feature in Hood River's downtown. Good examples of parapets are on the Hall Building (210-207 Oak.), the Sheppard buildings (First & State streets), Smith Building (213-15), Hood River Hotel (104-08 Oak), First National Bank (304 Oak), and HR Banking and Trust (Third and Oak).

Guidelines:

- A. Hide the roof plane from view by the use of parapets.
- B. Encourage the use of decorative details on the parapet using examples from surrounding buildings.
- C. Hide other HVAC, and other roof top equipment by the use of parapets.
- D. Use similar types of roofing material to others in the District on roofs that are visible from the adjacent streets or buildings.



Hood River Laundry, 413-15 Oak.



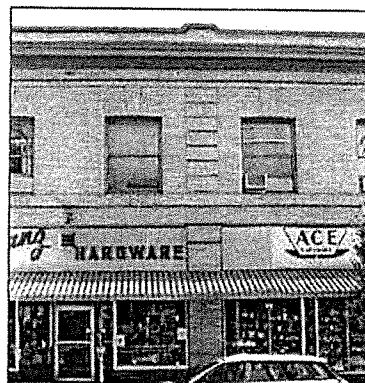
Hood River Garage, 202 Cascade.



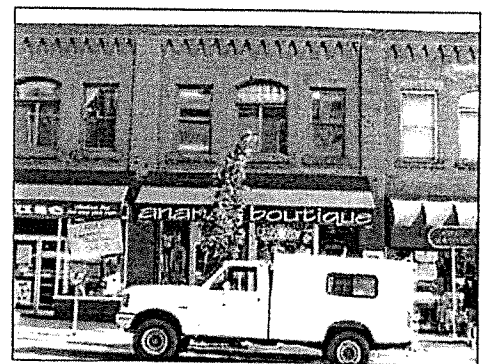
Highway Auto Company, 408-16 Oak.



Dewitt Motors, 107-113 Oak.



Franz Hardware, 116 Oak.



Brosius Building, 202-206 Oak.

Rehabilitation and New Construction Guidelines

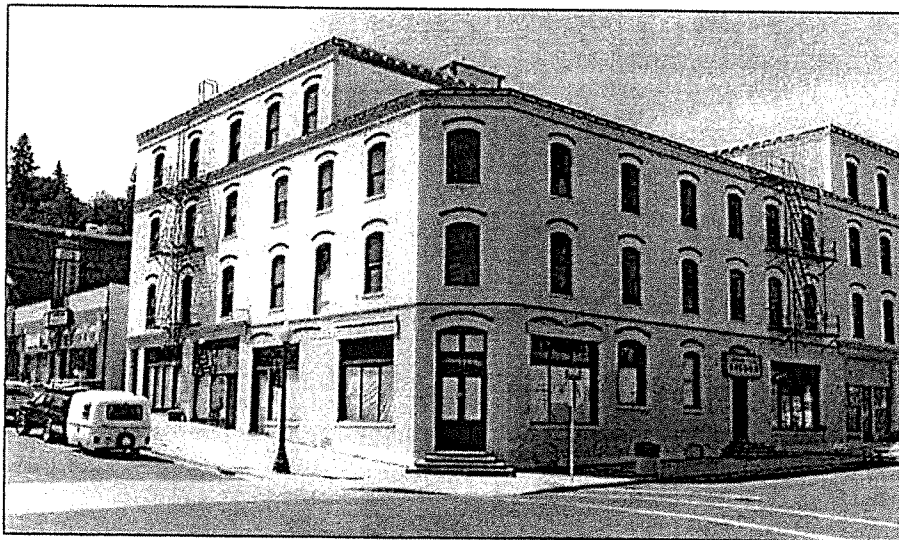
OPENINGS: Window Types and Proportions

The buildings in downtown Hood River were designed to house a variety of enterprises. These businesses often had central recessed entries that provided more window display space and shelter from the elements along with emphasizing the entrance from the sidewalk. Large display windows usually flanked the entries and low wooden, tile, or masonry bulkheads were built below the storefront windows. A band of horizontal transom windows were generally built above the storefront windows and entries. Historically, the entries were usually composed of single or paired doors made with large glass panes with wood surrounds.

The upper story windows in multi-story buildings are either paired or single, wooden double-hung windows which are vertically oriented. Generally these windows have enough space in between the windows for one or two window widths. The lower stories were often separated from the upper stories by a horizontal band created by such elements as a change in building materials, decoration, stringcourse, and/or awnings and canopies.

Guidelines:

- A. Recess primary entries and orient to the street rather than the side or rear.
- B. Use large panes of glass in the entry doors (paired or single). The use of a solid doors is not recommended on the primary or street facades.
- C. Incorporate transom windows above entries or expose existing transoms.
- D. Use large, clear plate glass in display storefront windows on street level with transom windows above and bulkhead below.
- E. Generally use double-hung windows either paired or singly for the upper floor windows.
- F. Maintain a clear visual division between the lower and upper stories by a change in material, surface texture, architectural detail, or use of awnings and/or canopies to define the horizontal division.
- G. Maintain the rhythm of the window pattern and the ratio of the solid surface area to the window area.
(Look at the surrounding building types to determine ratios of solid surfaces to window areas).



Hotel Waucoma, 106 Second. Displays common characteristics of 20th century commercial building in its recessed entrances, transom windows, clear division between lower and upper floors and one over one double hung windows.

Materials

The building material used in the construction of the historic buildings downtown was predominantly brick, hollow clay tile, and concrete. The sense of cohesiveness and continuity of the District derives in part from the consistent use of these building materials. The earliest buildings were constructed of brick; the later buildings are almost all concrete or hollow clay tiles. Common trim materials used historically include wood, brick, sheet metal, and concrete. The window sashes were constructed of wood on the earlier structures.

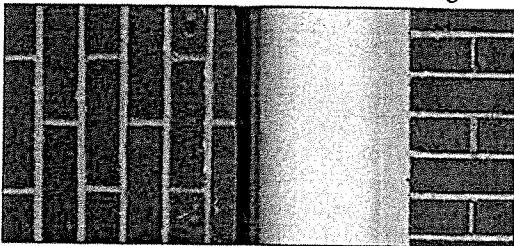
Guidelines:

- A. Use materials in rehabilitation projects and new construction that are compatible with the historic downtown in quality, color, texture, finish, and dimension.
- B. Encourage the use of wood windows.

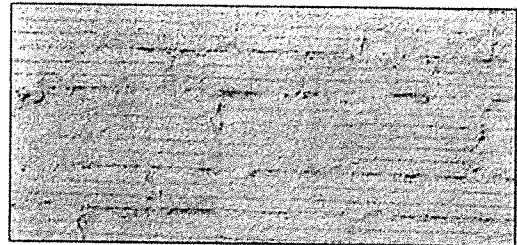
Avoid (These represent different building materials not generally found in Hood River)

- A. Vinyl siding or trim, vinyl windows, vinyl awnings
- B. Aluminum siding
- C. Wood siding consistent with residential construction
- D. Rustic wood shakes, barn wood
- E. Corrugated metal
- F. Corrugated fiberglass
- G. Imitation rock, wood, stone, or brick veneers
- H. Metalized reflective or "smoked" glass
- I. Glass block
- J. Wood shingle façade coverings or canopies

Building materials found in downtown Hood River



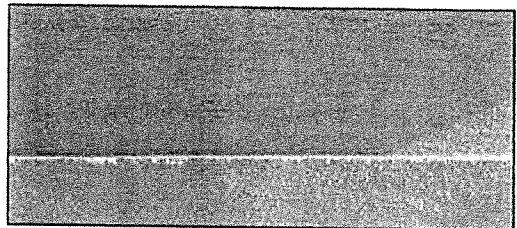
Brick and cast stone



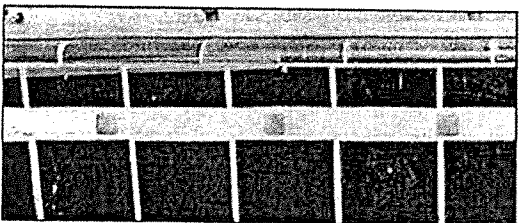
Hollow clay tile



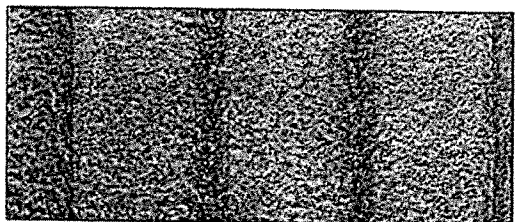
Stone used for trim and foundations



Concrete



Small ceramic tiles cover bulkheads



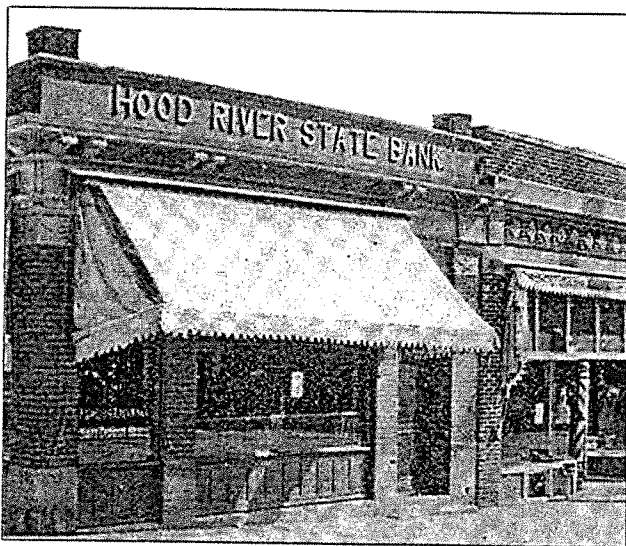
Stucco finish

Awnings and Canopies

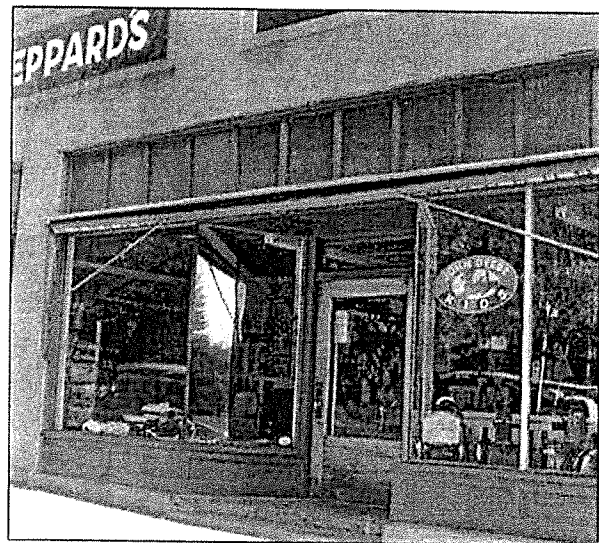
Awnings and canopies provide protection from the elements and create a sense of enclosure to the street. The historic photographs of Hood River show that awnings were plain in design, fit within the window opening or are outside the opening, retractable, and usually striped or solid in color (most likely white) with scalloped or straight edges. Awnings were mainly used on the north side of buildings in Hood River rather than the south side.

Guidelines:

- A. Use historic photographs for reference in replacing or adding new awnings or canopies. These photographs can illustrate the style and detail of historic awnings.
- B. Fit awnings and canopies (if possible) within window bays and do not overlap multiple window openings, unless historic photographs indicate otherwise.
- C. Awnings should not detract or conceal the architectural details or features of the building.
- D. Retractable fabric awnings were used historically and are recommended as they can be adjusted to varying light and weather conditions.
- E. Use an awning slope of no more than 45 degrees.
- F. Choose awning colors that are compatible with the color of the building. Avoid brightly colored or "busy" patterns.
- G. Use flat, horizontal metal canopies suspended by chains or rods, if original. These provide cover for pedestrians and shade within the store. Use historic photographs to aid in determining if the canopies are original or an early addition.
- H. Use of back-lit, plastic, barrel-shaped awnings are not recommended as they detract from the architectural features of the building and are more "modern" in appearance.
- I. Design lettering so that the writing is on the vertical edge and not on sloped or curved sections of the awning. Graphics or logos (without text) may be applied to the curved or sloped portions of the awning.



Original canvas awning on the Hood River Banking and Trust Building, 3rd and Oak.



Sheppard's (First & State streets): Good example of an original striped retractable awning intact below transom windows.

Color

Painting a storefront can be one of the most dramatic improvements to a building. The commercial buildings in the District are plain in design making them suitable for subtle color choices and a simple color scheme.

Guidelines:

- A. Conduct a paint analysis uncovering the original paint colors for the best method of designing a new paint scheme on a historic building.
- B. Harmonize and relate colors to the building material such as brick or wood.
- C. Blend the color choice with buildings in the same block.
- D. Choose a color that is similar to the original brick color on buildings that have been previously painted.
- E. Choose a maximum of three colors to highlight any building façade. Do not over decorate.
- F. Tie together the upper and lower floors on multi-level buildings by using the same trim color.

Avoid:

- A. Avoid using intense hues and a number of vivid colors on the building.
- B. Avoid colors that highly contrast or overly accent decorative details on the buildings.
- C. Avoid painting unpainted brick.
- D. Avoid sandblasting or using harsh chemical cleaners to remove paint from brick; conduct a test patch prior to cleaning.

When painting a building the following scheme is generally recommended:

darkest color for the window sash; medium color for the body of the building; and lightest color for the trim.

